URBAN DESIGN REPORT - PLANNING PROPOSAL

FOR NO. 252-254 NEW SOUTH HEAD ROAD, DOUBLE BAY

March 2020

GMU
Urban Design & Architecture

GMU
Urban Design & Architecture

Prepared by GM URBAN DESIGN & ARCHITECTURE PTY LTD
Studio 803, Level 8
75 Miller Street
North Sydney NSW 2060

Tel (02) 8920 8388

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Nominated Architect - Ms Gabrielle Morrish
Registered Architect with NSW Architects Registration Board - Reg Number 5572
Registered Architect with Architects Registration Board UK - Reg Number 060492E

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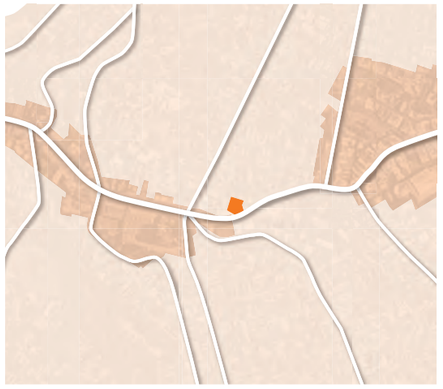

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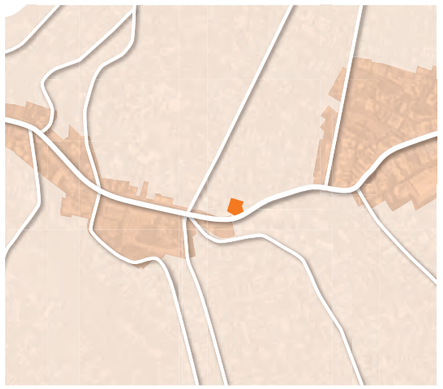
1. INTRODUCTION



1.1 INTRODUCTION

GM Urban Design and Architecture (GMU) have been appointed by Penoh Capital Land Pty Ltd to undertake an independent Urban Design Study for the future development potential of the subject site located at No. 252-254 New South Head Road, Double Bay. The study has been prepared to inform a Planning Proposal for the redevelopment of the subject site.

GMU's report presents an analysis of the subject site within its broader strategic & local context to ensure the built form strategy considered for the site, responds appropriately to the existing and future desired character of the surroundings.

The main purpose of this study is to develop built form principles to inform a better built form outcome, consistent with the future growth of Edgecliff, Double Bay and the wider area. We consider the proposed massing will also maintain and enhance the character of the site as well as the local area.

Our study has considered the broader planning framework for the area including the Eastern District Plan by the Greater Sydney Commission, recent studies, recent approvals in the area and the underlying objectives of Council's controls.

In preparing this report, GMU have worked with the following consultant team:

Town Planners - GSA

Architects - Antoniades Architects

Traffic Consultant - TTPA Traffic

Arborist Consultant- Advanced Treescape Consulting

Perspective Artist - Antoniades Architects

Methodology

In undertaking this study, GMU have conducted extensive analysis to understand the subject site within the context. In arriving at the views and conclusions expressed in this report, GMU have:

- Visited the site and its immediate and broader context.
- Reviewed Greater Sydney Region Plan - A Metropolis of Three Cities.
- Reviewed Our Greater Sydney 2056 - Eastern District Plan.
- Woollahra Local Strategic Planning Statement (Draft September 2019).
- Reviewed the current development controls under Woollahra LEP 2014 and DCP 2015 for the site and the wider area.
- Reviewed Council's website and information about the area.
- Council's Pre-Application Consultation response, 15th September 2019.
- Reviewed recent development applications and Planning Proposal approvals in the vicinity of the subject site.
- Considered the site survey by project surveyors.
- Reviewed the planning advice by GSA Planning.
- Reviewed the Traffic and Parking Impact Assessment by TTPA.
- Reviewed the Arboricultural Impact Assessment by Advanced Treescape Consulting.
- Reviewed the indicative sheme by Antoniades Architects.
- Analysed the site, desired future character intended for the surrounding context, opportunities and constraints of the subject site and its immediate existing context.
- Created urban design principles and height strategies for the subject site.
- Coordinated with the project team to provide on-going advice in the formulation of design guidelines for the indicative architectural scheme and the preferred masterplan.

We have undertaken an evidence-based approach to urban design analysis, the streetscape and the wider area to understand the existing and future urban fabric and the role of the site within it.

As discussed in this report, GMU and the project team have also undertaken testing of massing options to arrive at a place-based strategy for the site, taking into account the site constraints and key character elements of the area.

Figure 1. The subject site, looking north from the driveway of No. 250.

1.2 THE SITE

The subject site is located on New South Head Road within the R3 (Medium Density Residential) zoned lands in Double Bay. It is in proximity to both Edgecliff and Double Bay Local Centres. The site is irregular in shape and has a total area of approximately 934.9 sqm. The site has a frontage of approximately 19 metres to New South Head Road.

It is currently occupied by a 4-storey residential flat building with a tiled roof. The surrounding properties predominantly include residential, commercial, educational uses and as well as community services.

The site has good connectivity to and from the surrounding areas. It is located within walking distance of a number of facilities, including:

- Bus services along New South Head Road (approximately 2-min walk)
- Double Bay Wharf and Steyne Park (11-min walk)
- Trumper Oval, Park and Tennis Centre (11-min walk)
- A range of community facilities (400-800 walking catchments)
- Coles Supermarket located in the Edgecliff Centre and a range of retail offerings (4-min walk)
- Ascham School & Double Bay Public School (4-10-min walk)
- Double Bay Village and Shops (approximately 350m)
- Martin Place Train Station (approximately 5-min via train)
- Edgecliff Station (approximately 5-min walk)
- Edgecliff Bus Interchange (approximately 5-min walk)

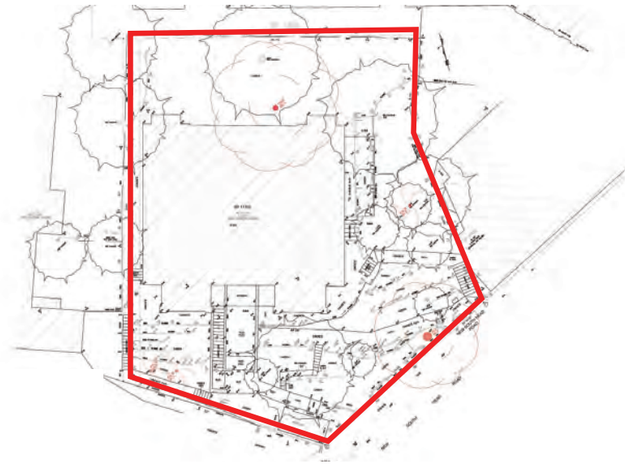


Figure 2. Site Survey (Cibar Surveying Pty Ltd dated 23.09.19).

Figure 4. The subject site viewed from New South Head Road. The driveway to No. 250 is visible in the foreground (Google Streetview).

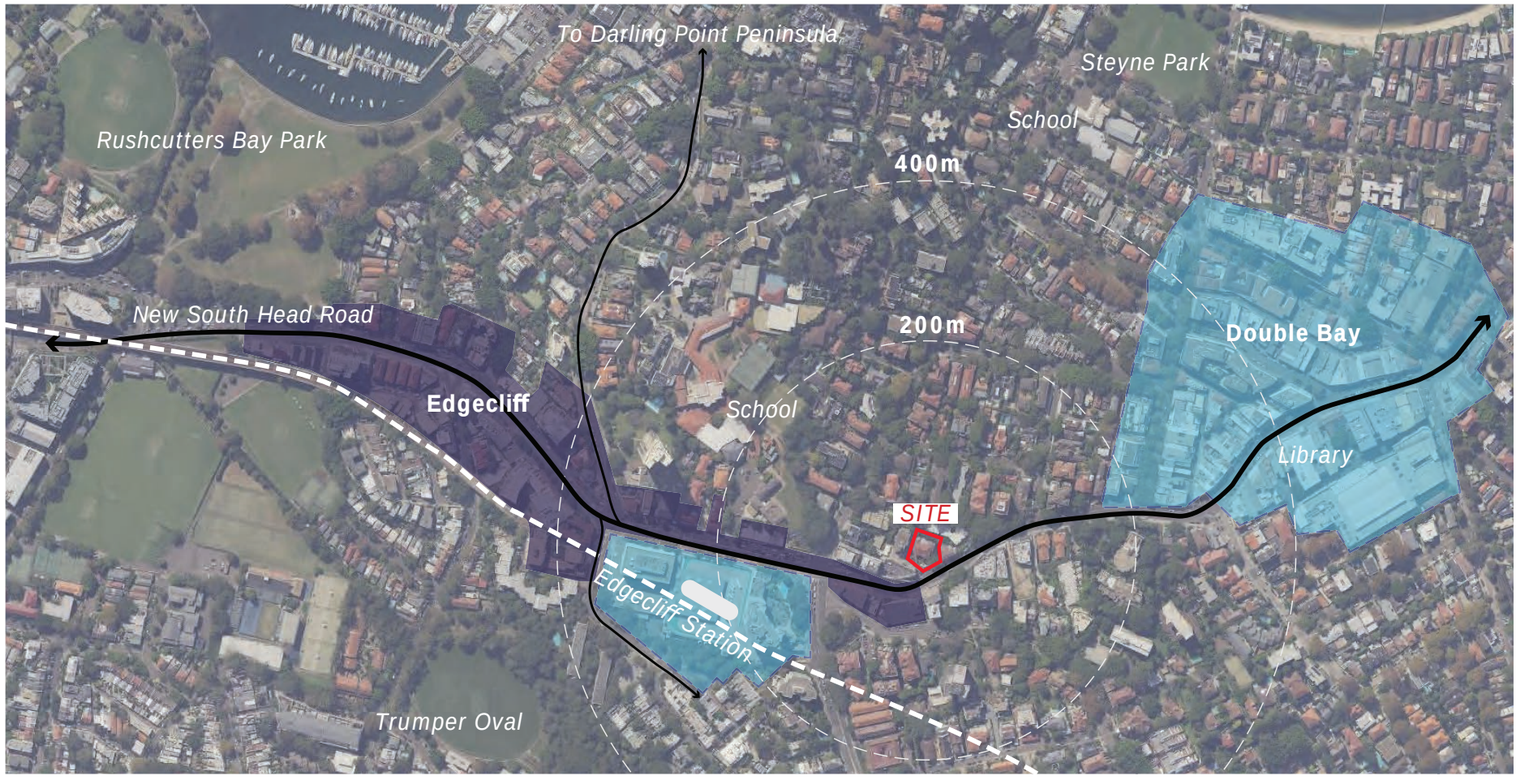


Figure 3. Aerial photo showing the site in context. B2 Centre Edgecliff Mixed Use Corridor (B4) The subject site

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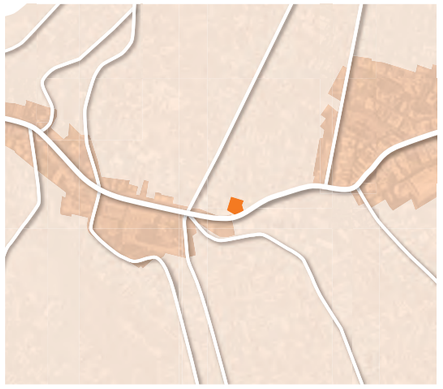
2. STRATEGIC CONTEXT ANALYSIS



2.1 EDGECLIFF WITHIN THE STRATEGIC CONTEXT

This chapter analyses the role of Edgecliff, Double Bay and surrounding areas within the broader strategic framework to understand the direction the NSW State Government intends for the locality.

A Metropolis of Three Cities

The plan aims to achieve a '30-minute' City where people can live and work within 30-minute distance of the strategic centres and job hubs using primarily public transport for daily commutes.

The area is located less than a 30-minute distance from strategic centres and major job hubs including Sydney's CBD, Bondi Junction and the Randwick Health and Education Precinct.

From Edgecliff Train Station, the journey to Sydney's CBD and Bondi Junction takes approximately 5 minutes by train. Train services are frequent.

The site is located within short walking distance from Edgecliff Train Station and bus interchange and it is well placed to provide housing close to a transport node to meet the strategic aims of A Metropolis of Three Cities.

The Eastern City District Plan

The Eastern City District Plan identifies Edgecliff as a Local Centre in proximity to Double Bay which is also nominated as a Local Centre. Bondi Junction is identified as a Strategic Centre and is located approximately 2km south of the site. Edgecliff centre is identified as having potential housing, retail and commercial growth opportunities within 800m walking catchment focusing on its public transport node. The subject site is located approximately 200m to the east of the transport node.

Planning Priorities:

- Creating and renewing great places and local centres, and respecting the District's heritage
- City supported by infrastructure
- Housing Supply and Choice

Principles for local centres:

- *Will be appropriate to accommodate additional housing as part of their housing strategy.*
- *Will need to grow to provide for the required goods and services of the community.*
- *May also need to grow to deliver other roles for the community, such as recreation, cultural, arts and community hubs.*

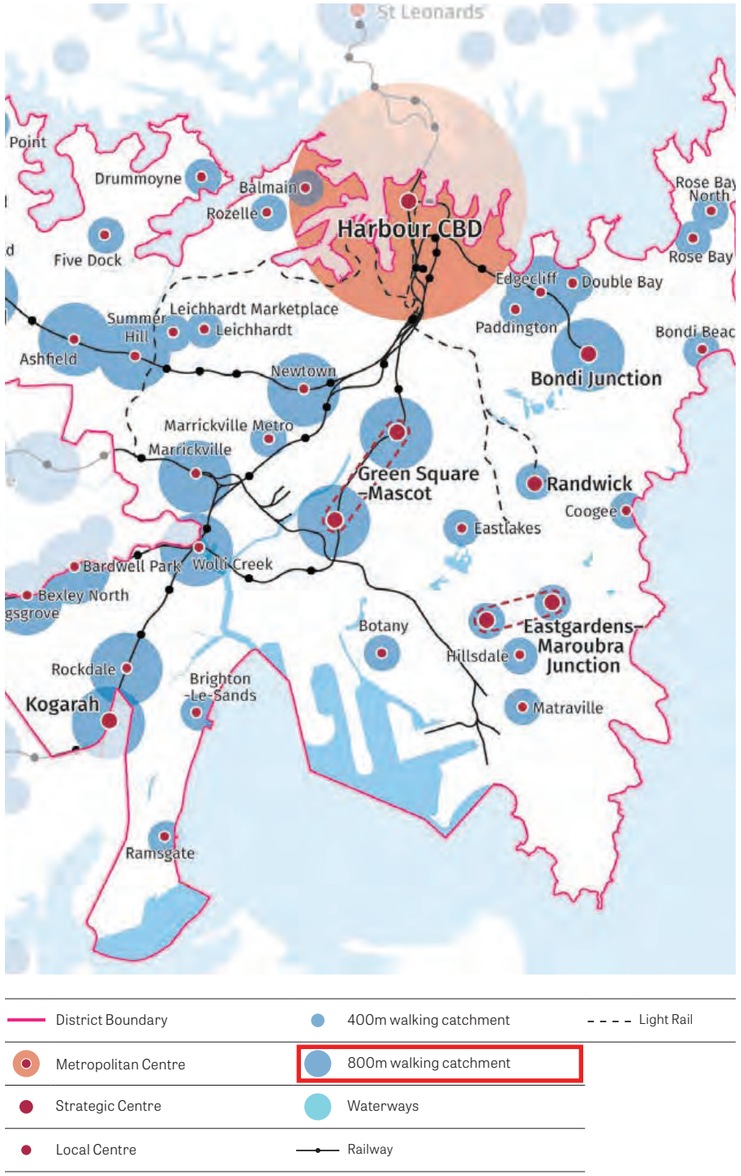


Figure 5. Eastern District Plan centres (Eastern City District Plan, March 2018).

The Eastern City District Plan identifies local centres as playing an important role in providing local employment and community facilities. In addition to housing, the centre's primary role is to provide goods and services, and the opportunity for growing its employment function over time.

The Eastern District Plan identifies assets such as Sydney Harbour, the eastern beaches, Centennial Park, creative and night-time activities of Sydney's CBD, Darlinghurst/Kings Cross/Potts Point as the prominent tourist destinations in the district. This is one of the reasons why the area is a well-known location along with the scenic qualities and open space amenity of the area.

Edgecliff Centre is a well-established local centre located along New South Head Road, which is a state arterial road in proximity to public open spaces, parks and waterfront areas. The area provides the retail/commercial strip for the local community and the general public.

The Centre is strategically located close to:

- Sydney's CBD and other strategic centres including Bondi Junction and the Randwick Health and Education Precinct.
- Local and international destinations including the eastern beaches and cultural centres.
- Job hubs, educational/community facilities, services and public transport corridors.

This provides an opportunity to contribute to the future growth of the district and revitalise the local centres by creating additional infrastructure such as services and accommodations as predicted by the strategic policies.

The subject site is located within walking distance of the Edgecliff Centre, the train station and the bus interchange which reinforces the District Plan's aim for a '30-minute city' and supports sustainable public transport use.

As part of our strategic review, GMU have reviewed the Draft Local Strategic Planning Statement (LSPS) published by Woollahra Council in September 2019.

Woollahra Local Strategic Planning Statement

The document sets out a 20-year land use vision, nominating a series of planning priorities for the LGA. The document includes a local vision to guide future development, nominating local planning priorities and associated strategies and actions. Planning priorities set out in the LSPS include Infrastructure and Collaboration along with an ambitious active transport vision nominated for the area. We understand Council is seeking to implement the following strategies to further the transport network including:

- Continue to encourage use of active transport;
- Deliver improved cycle way connections;
- Advocate for increasing the role of Edgecliff as a key transport in the area;
- Promote increased walkability and pedestrian safety.

GMU understand that Council advocates for improved connections and interchange upgrades to improve the connectivity of the area and continue to provide convenient access to Sydney's CBD and other major job hubs.

The subject site is located approximately 200m from the Edgecliff Centre, conveniently located to facilitate housing consistent with Council's nominated planning strategies.

Planning priorities for livability focus and emphasize the following:

- Diverse range of housing choices in well-planned locations with easy access to high-frequency public transport, local villages, job services, and open spaces;
- Diverse housing choices in planned locations that enhance our life styles and fit in with local character and scenic landscapes.

The subject site is well-placed for providing development in accordance with the nominated priorities for the area due to its proximity to public transport, village locations and the site and context's natural features.

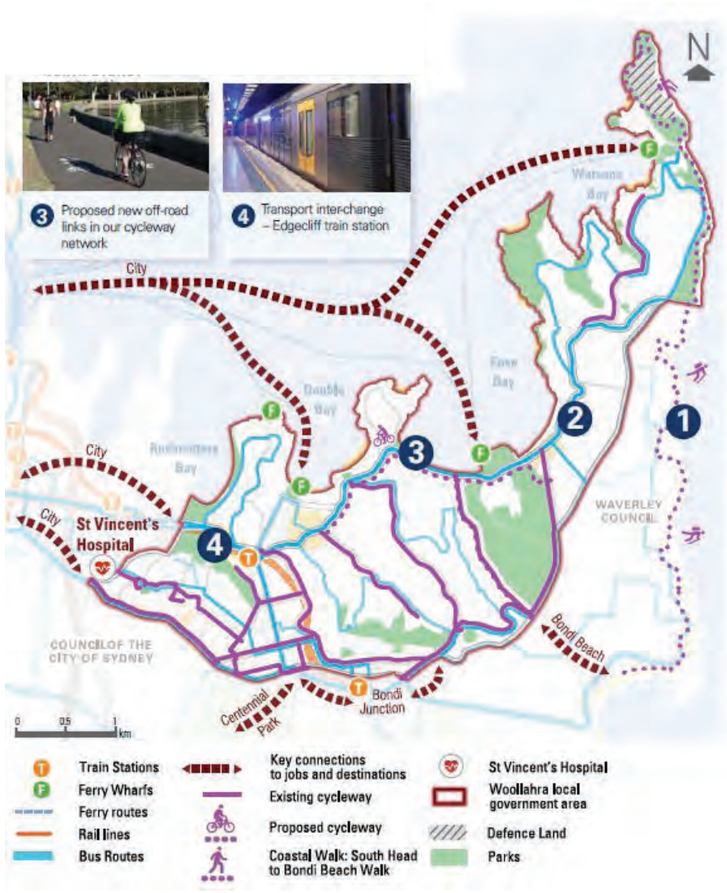


Figure 6. Connectivity diagram (Woollahra LSPS).

Figure 7. Structure Plan (Woollahra LSPS)

The Structure Plan (Figure 7) nominates Edgecliff and Double Bay as the Key Local Centres within the LGA which is also consistent with the centres hierarchy nominated in the Eastern City District Plan. The subject site is located within walking distance of both local centres. The Structure Plan also shows the Edgecliff Centre as having the only rail connection within the LGA. Edgecliff bus interchange is also located within the Edgecliff Centre site. Within minutes, commuters can travel by train from Edgecliff to Sydney's CBD.

2.2 TRANSPORT LINKS AND URBAN PROFILE

Development heights along the eastern railway corridor

Similar to other centres, development heights in Edgecliff and Double Bay are likely to increase over time. To understand the urban profile of the area it is important to consider the existing heights along the railway corridor.

The height and urban structure characteristics of numerous Sydney transport corridors show a height concentration of 10-27 storeys around the centres and along the transport corridors. The future urban profile of Edgecliff should respond to the character of the CBD edge and be informed by the proximity to major job hubs in accordance with the strategic aims. We believe proximity should be considered as well as travel time using public transport. Development heights in Double Bay should transition to lower scale developments further east.

The main link from the city for vehicular movements through Kings Cross is William Street, the road underpass and Victoria Street. William Street has been reinforced with a variety of scales from 3-4 storey heritage items as street wall buildings to high rise towers of 100m (33-35 storeys). Upon approach to Kings Cross, a tower cluster clearly demonstrates the entry to Kings Cross and Darlinghurst with towers in the order of 50-110m (15-35 storeys), both over the station and on the intersection corners. The scale reduces to buildings of 22-40m (6-11 storeys) along William Street. The transition from Elizabeth Bay/Kings Cross is punctuated by Rushcutters Bay Park and lower development along Neild Avenue. The approach to Edgecliff & Rushcutters Bay is announced by two taller buildings (up to 32-storeys). As discussed later in this report, existing development heights vary along New South Head Road. East of Bay Street developments are generally lower scale (2-5 storeys) at the lowest point of the topography.

Increased heights tend to be focused on or close to the ridge line as seen in figure 8. Based on street hierarchy, height profile and a height range of 20-70 storeys from Hyde Park towards the city, 12-20 storeys occur along William Street and up to 35 storeys on Kings Cross.

Future heights for the Edgecliff Centre would be modest if anticipating a height range of 18-20 storeys. This range still maintains its lower centre hierarchy compared to other major centres along the Eastern Railway Corridor. Development heights are likely to transition to the edges of centre areas along New South Head Road where the subject site is located. The subject site and surrounding area is likely to provide heights of 4-8 storeys (fronting New South Head Road). Later chapters of this report examine development heights in greater detail.

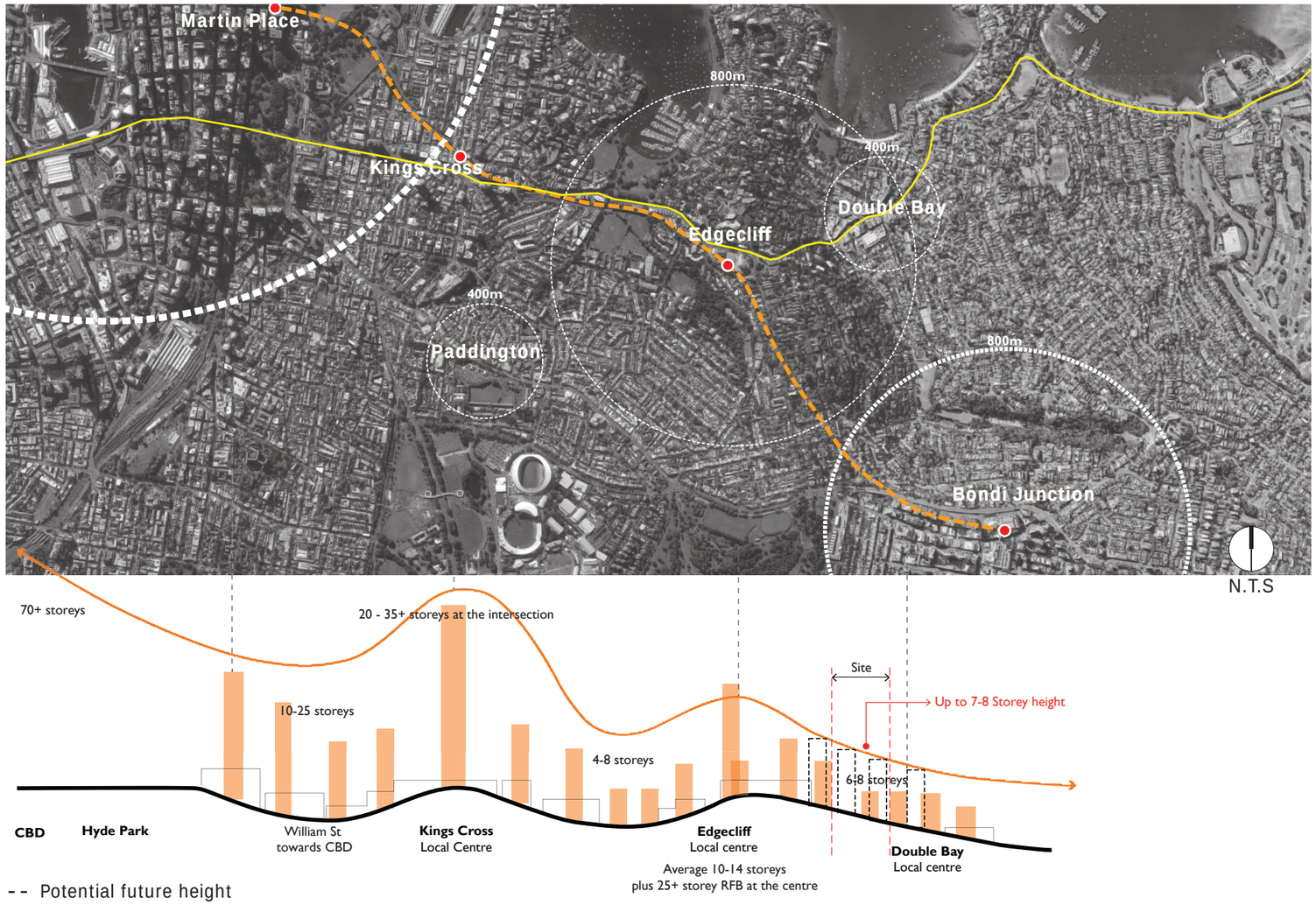


Figure 8. Height profile diagram.

2.3 LEP CONTROLS

GMU have reviewed the current applicable controls for the area and the site. The applicable controls are discussed below including zoning, height of building and FSR. We understand the site is not located within a heritage conservation area nor does it contain any heritage items.

The subject site is located within the R3 (Medium Density Residential) zone along New South Head Road. The site is in proximity to the B4 (Mixed use) zone in Edgecliff as well as the B2 (Local centre) zones.

ZONING

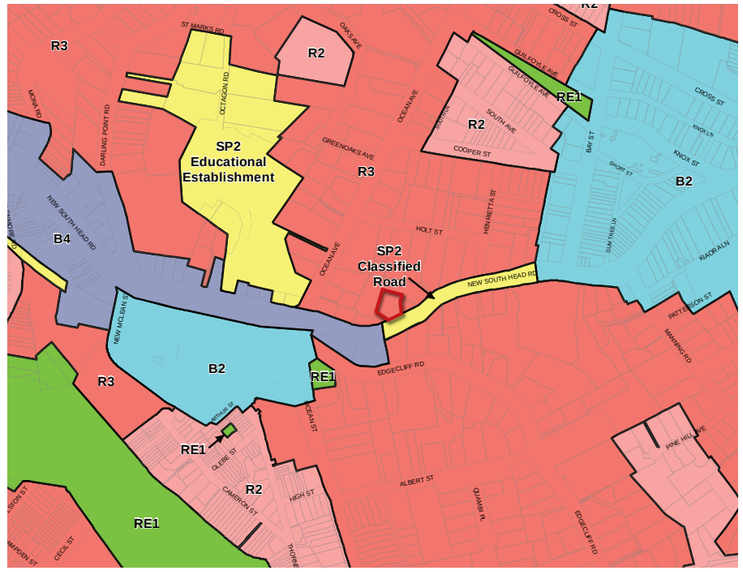


Figure 9. LEP Zoning Map (Woollahra LEP 2014 - Map 003).

The objectives for the R3 - Medium density residential zone as per the LEP 2014 aim:

- *To provide for the housing needs of the community within a medium density residential environment.*
- *To provide a variety of housing types within a medium density residential environment.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*
- *To ensure that development is of a height and scale that achieves the desired future character of the neighbourhood.*

HEIGHT OF BUILDING

C	5	I1	8	K	10.5	N2	13.5	O2	16.5	Q2	19.5	T	26
E	6	I2	8.2	L1	11	N3	14.1	P1	17.5	Q3	20.5	U	34
F	6.5	J1	9	L2	11.5	N4	14.5	P2	18	R1	21.5		
G	7	J2	9.5	M	12	N5	14.7	P3	18.1	R2	22.5		
H	7.5	K	10.5	N1	13	O1	15	Q1	19	S	23.5		

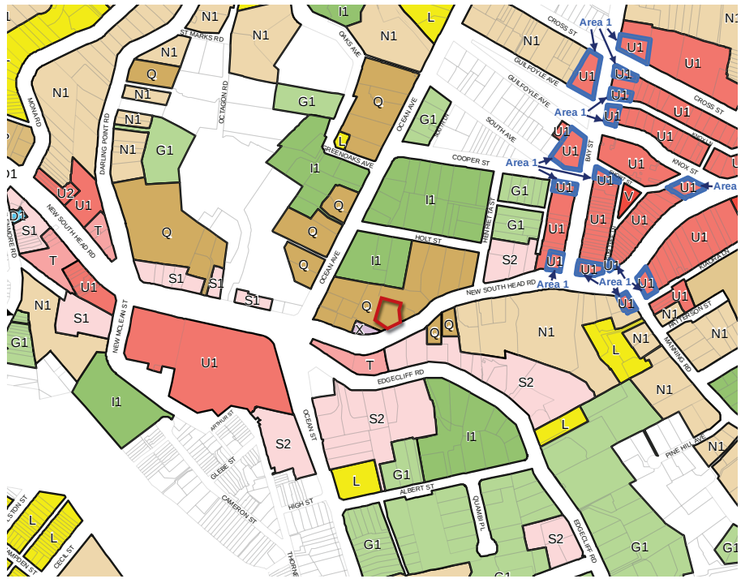
Figure 10. LEP HOB Map (Woollahra LEP 2014- Map 003).

The aims and objectives for height of buildings as per Cl. 4.3 in WLEP 2014 aim:

- *To establish building heights that are consistent with the desired future character of the neighbourhood.*
- *To establish a transition in scale between zones to protect local amenity.*
- *To minimise the loss of solar access to existing buildings and open space.*
- *To minimise the impacts of new development on adjoining or nearby properties from disruption of views, loss of privacy, overshadowing or visual intrusion.*
- *To protect the amenity of the public domain by providing public views of the harbour and surrounding areas.*

A maximum building height of 13.5m currently applies to the site.

FLOOR SPACE RATIO



G1	0.65	L	0.9	Q	1.3	S3	1.68	V	3
G2	0.68	N1	1	R1	1.4	S4	1.7	X	4
H	0.73	N2	1.04	R2	1.42	T	2		
I1	0.75	O	1.1	S1	1.5	U1	2.5		
I2	0.77	P	1.25	S2	1.55	U2	2.9		

Figure 11. LEP FSR Map (Woollahra LEP 2014 - Map 003).

The aims and objectives for the floor space ratio as per Cl. 4.4 in WLEP 2014 aim:

- *To ensure the bulk and scale of new development is compatible with the desired future character of the area.*
- *To minimise adverse environmental effects on the use or enjoyment of adjoining properties and the public domain.*
- *To ensure that development allows adequate provision on the land for deep soil planting and areas of private open space.*

A maximum FSR of 1.3:1 currently applies to the site. GMU understand that a number of recent amendments to the LEP provisions has resulted in increased maximum density controls. These include:

- Nos. 240-246 New South Head Road - FSR 4:1
- Nos. 80-84 and 90 New South Head Road - FSR 2.9:1
- No. 374 and 376-382 New South Head Road - FSR 4.5:1

2.4 CURRENT APPLICABLE DCP CONTROLS

The key current development controls applicable to the subject site are summarised as follows:

DCP B1.3 Double Bay Precinct

This part of the DCP provides a Character Statement, Desired Future Character Statement and objectives to guide development within the precinct and ensure key character elements are appropriately considered.

B1.3.1 Precinct character statement

The character statement includes the following character observations:

- The Double Bay precinct has a strong feeling of community and convenience due to the facilities located within the precinct and the proximity of the adjacent town centre.
- The precinct sits within a low lying basin, framing the Double Bay foreshore.
- The precinct is notable for both the density of its built form and its green landscape character. The tree canopy, formed by both street trees and private plantings, is a prominent element in the municipality's presentation from Sydney Harbour and should be maintained.
- The curvilinear sequence of New South Head Road which forms an edge to the precinct to the south.
- Due to the topography and location, major streets present unique settings within the broader common elements of the precinct.

B1.3.2 Desired future character:

The Character Statement includes the following points:

- On sloping land, new development should step down the site to reinforce the topography.
- Attention must also be given to protecting views, particularly views from public spaces down Ocean Avenue, Bay and Beach Streets to the harbour, and views from New South Head Road to the harbour.

A number of streetscape character and key elements are nominated for the precinct including:

- The rich mixture of architectural styles and building forms.
- The topography and the siting of development on New South Head Road, which descends into (and ascends out of) the Double Bay centre.
- The established tree canopy.

This chapter also provides objectives for future development in the precinct and the desired future character to:

- Respect and enhance the streetscape character and key elements of the precinct.
- Reinforce a consistent building scale within streets.
- Ensure that rooflines sit within the predominant street tree canopy.
- Provide a transition between the higher density buildings of the Double Bay centre and the lower density buildings of the residential area.
- Retain and reinforce the green setting of mature street trees, private trees and garden plantings.
- Protect important iconic and harbour views from the public spaces.

While Chapters D2.2 and D3 of the DCP do not strictly apply to our site. Provisions apply to areas immediately to the west along the New South Head Road Corridor and we have therefore studied the provisions as part of our initial investigations.

Chapter B3.2 outlines setback requirements including:

- Front setback C1. The front setback of the building envelope is determined by averaging the three most typical setbacks of the four closest residential buildings, facing the same side of the street.
- Side Setback C2. The minimum side setback for Residential Flat Buildings, attached dwellings and multi-dwelling housing is determined by the site width meassured along the front setback line as determined in Figure 5B (below).
- Rear Setback C1. The rear setback is a consequence of the site depth, front setback and building depth as set out in the formula at Figure 6 (below).

FIGURE 5B
Side setback table for Residential flat buildings, multi dwelling housing and attached dwellings, and any other land use not addressed in controls C1 to C2 of Section 3.2.3 Side setbacks

A. Site width measured along front setback line in metres	B. Side setback in metres
<18.0	1.5
18.0 - < 21.0	2.0
21.0 - < 28.0	2.5
28.0 - < 35.0	3.0
35.0 +	3.5

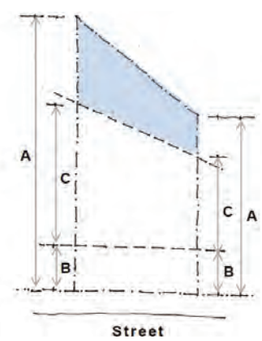


FIGURE 6
Formula for determining the rear setback

Rear setback = A – C – B

A = Site depth

B = Front setback

C = Building depth (A x % for A on the building depth sliding scale)

Figure 12. Setback control diagrams (WDCP Chapter B3).

Later chapters of this document discuss the proposed urban design principles and the preferred massing concepts for the subject site. This report summarises the proposal's response to the aims and objectives of the applicable controls to ensure that the proposed development provides a harmonious response to the existing context and considering the future desired character of the area in accordance with Council's policies and strategic plans.

In addition to the precinct-specific controls, a number of general development controls are found within the DCP Chapter B3. These are summarised below. B3.2 provides the following setback controls for residential flat developments in R3 zone:

B3.8.6 Residential flat buildings and multi dwelling housing

Chapter 3.8.6 applies to residential flat buildings and includes the following objectives and controls:

- To ensure usable and well located areas of private open space that provide good amenity for residents.

Objectives

- To ensure that dwellings within the development provide good amenity.
- To ensure usable and well located areas of private open space that provide good amenity for residents.

Controls

- Internal layout and window placement achieve good natural ventilation.
- Single aspect dwellings are limited in depth to 8m from a window.
- The back of the kitchen is no more 8m from a window.
- The width of a cross-over or cross-through dwelling over 15m deep is 4m or greater. Deep and narrow dwelling layouts are avoided.
- Where practical, habitable rooms excluding bedrooms are oriented to the north for maximum solar access.
- Light wells as the main source of lighting and ventilation to dwellings is avoided.
- Each dwelling has direct access to its own private open space area.
- Private open space areas are located and designed to minimise overlooking from other dwellings in the development.

SEPP 65 and the Apartment Design Guidelines (ADG) apply to residential flat developments of more that two storeys and 4 units.

2.5 OPPORTUNITY SITES IN EDGECLIFF

Woollahra Council has identified 26 locations as 'opportunity sites' (refer opportunity sites document dated June 2010) to increase dwelling capacity and meet the housing targets set out by the NSW Government in the East Subregional Strategy in 2010.

The sites were identified following critical planning analysis of all land within the Woollahra Local Government Area (LGA), and are estimated to yield 1,000 new dwellings. Each of these sites has an increased development potential through increased maximum building height and floor space ratio and in some instances by rezoning to allow residential flat buildings or mixed use developments.

The study focused on planning changes for the business centres and immediate surrounding areas, particularly Edgecliff and the New South Head Road corridor to Rushcutters Bay consistent with:

- good planning practice
- more sustainable and transport oriented development, and to
- Protect the character and amenity of Woollahra's low density residential areas by limiting the need for significant change to the planning controls in these areas.

The sites identified were in Edgecliff, Vaucluse, Rose Bay, Bellevue Hill, Paddington, Woollahra and Double Bay.

For the Edgecliff Centre, 4 sites were identified including:

- 1. Edgecliff Centre: 203-233 and 235-285 New South Head Road
- 2. Western Gateway:73-79 New South Head Road (service station site)
- 3. Western Gateway: 2-14 New South Head Road
- 4. Eastern Gateway: 240-246 New South Head Road

These sites are identified on the adjacent diagram.

Of the identified sites, the eastern gateway site (No. 4) which is located to the west of the subject site has recently been completed. The height of building is 18m with a secondary height of 14m at the highest part of the site (Area H).

Figure 13. The location of opportunity sites within the area identified as Edgecliff Mixed Use Town Centre.

The key justifications for planning changes for each of the sites located close to the subject site are summarised below:

1- Edgecliff Centre: 203-233 and 235-285 New South Head Road

- The need to increase the development potential in centres to promote more sustainable and public transport oriented development.
- Potential to meet 40% of the growth required.
- Protection of residential areas from significant change.
- The benefit of uniting two sites that currently function separately, improving the retail experience and customer amenity.
- An increase to the site's FSR up to 6.05:1.
- An increase to the site's height up to 53m (17 storeys).

4- Eastern Gateway: 240–246 New South Head Road, Edgecliff

- An increase to the density within 400m of Edgecliff Centre.
- More sustainable and public transport oriented development.
- Zoning change to B4.
- An increase to the site's height up to 24.9m (part 6 to 7 storeys).
- An increase to the site's FSR up to 4.8:1.

We understand the site has been approved with a maximum FSR of 4:1 and a height of 18m through the planning proposal process.

Other than the site located at the eastern gateway, the remaining three sites have been deferred by Council subject to further investigation and 'strong and supportable reasons'. We understand that Council is currently undertaking an extensive analysis of both local centres which includes these opportunity sites in Edgecliff. However, the conclusions of the analysis have not been advertised as yet.

It is important to understand the main reasons for why selected specific sites were nominated for future growth, focusing on transit oriented development and accentuating prominent locations such as arrival points and core areas.

The subject site is located immediately to the east of the eastern gateway site (No. 4). This should be considered as part of the analysis of the existing and future height profile and densities along New South Head Road to ensure harmonious streetscape proportions.

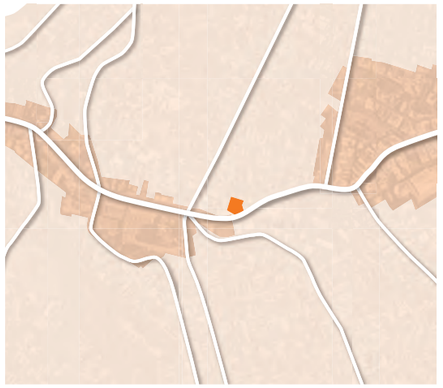
2.6 SECTION CONCLUSION

The strategic direction of the Eastern City District Plan is to capitalise on strategic and local centres as well as local and major public transport nodes, in order to locate additional density including housing, jobs and services to achieve the concept of the 30-minute city.

This approach is consistent with Woollahra Council's strategic priorities formulated within the LSPS (draft) where increased connectivity and easy access to high-frequency public transport, open space amenity and the retention of the local village setting is emphasised through selected planning priorities.

The subject site is ideally located to offer housing choice close to local centres and public transport, taking advantage of the quality lifestyle offerings associated with the locality and the proximity to open space amenity, village centres, scenic landscapes and key community destinations.

3. LOCAL CONTEXT ANALYSIS



3.1 KEY CHARACTERISTICS OF THE LOCAL AREA

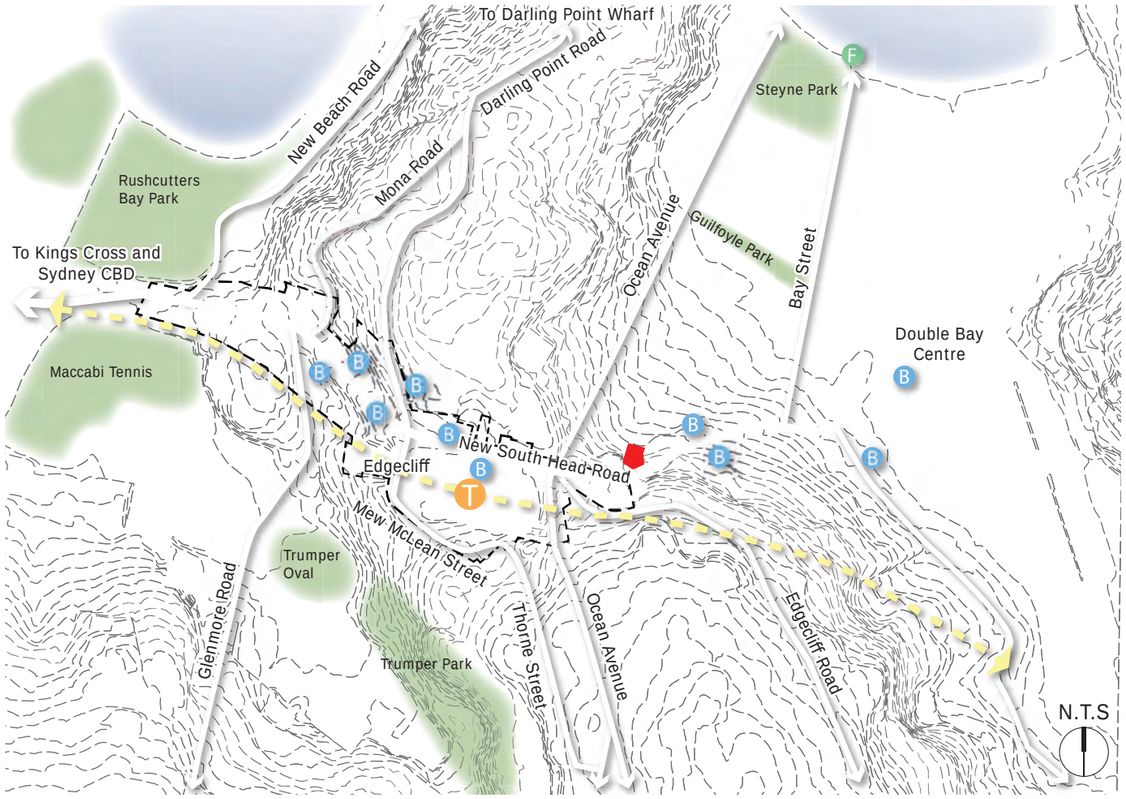


Figure 14. Topography and connectivity.

The site | Local parks | Streets | Railway corridor
Edgecliff Station | Ferry wharf | Bus stops

TOPOGRAPHY AND CONNECTIVITY

The site is located in Double Bay at the edge of the Edgecliff Centre. Edgecliff is the gateway to the eastern suburbs and is well connected to the City and surrounding suburbs. It is well connected through the existing urban street structure and numerous public transport services including bus, train and ferry services at Double Bay Wharf located approximately 800m to the north-east of Edgecliff Centre. The main bus corridor along New South Head Road and Ocean Street connects Edgecliff to Sydney CBD, Centennial Park, Watsons Bay and Bondi Junction.

The subject site is located approximately 200m to the east of the Edgecliff Train Station, close to bus stops which provide frequent access to the city, Bondi Junction and local destinations. The site is located immediately to the east of the Edgecliff Local Centre area as shown in Figure 14 above.

The centre area follows the alignment of New South Head Road and has distinctive topography where the core is located towards the top of the ridge. This provides an opportunity for extended city, district and water view as well as marking the centre.The topography is unique to the area, adding to the character of the villages and the relationship between them. Edgecliff is located on the ridge line whereas Double Bay is located in the valley further east. The subject site is located at the edge of the Edgecliff centre where the sloping terrain descends towards Double Bay and the finer grain subdivision pattern. This is illustrated in Figure 15.

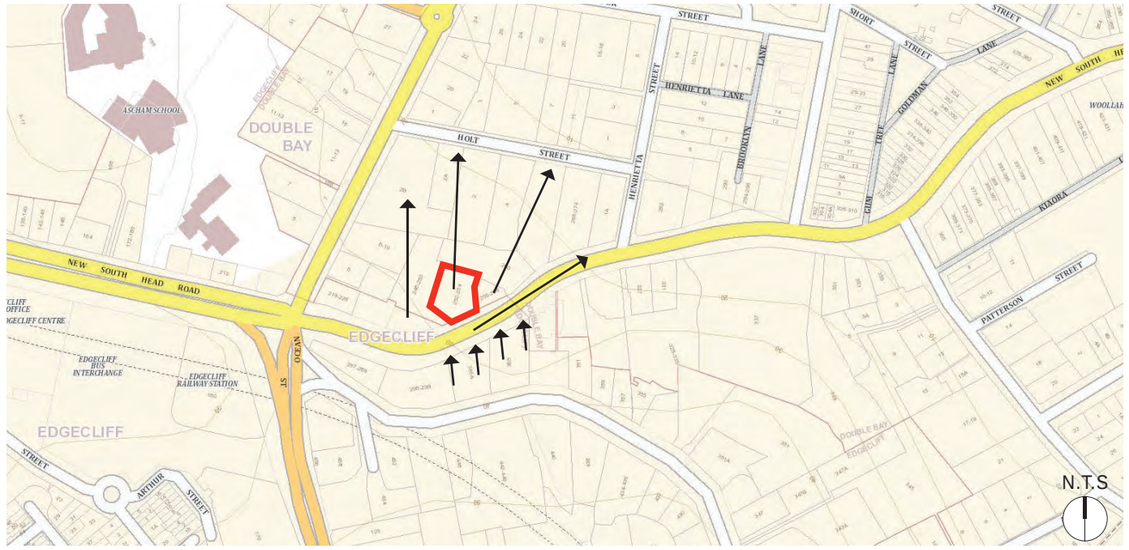


Figure 15. Map adapted from SixMaps showing the variation in the lot subdivision pattern and the topography.

The site fronts New South Head Road where the curvilinear sequence of the road changes alignment, descending to Double Bay centre. This is a characteristic feature of the streetscape within this particular part of the New South Head Road. Furthermore, the terrain also slopes to the north creating a unique setting and a characteristic streetscape profile. Several lots along the southern side of the road are elevated above the footpath whereas lots along the northern part of the road descend with the built form located below the level of the footpath. This is illustrated in the photographs below and streetscape characteristics are further discussed under chapter 3.5 Immediate Context.

Figure 16. Photographs showing the topography and lots fronting New South Head Road

3.2 KEY CHARACTERISTICS OF THE LOCAL AREA

Figure 17. Local destinations and amenity.

Key

The site | Local parks | Streets | Railway corridor

Educational facilities | Local/community Facilities | Edgecliff Mixed Use Centre

LOCAL DESTINATIONS AND AMENITY

As shown in Figure 16, the site is located in proximity to a number of local destinations and community facilities within easy access via the existing roads and public transport services.

Local attractions include Rushcutters Bay and Double Bay Beach, Double Bay Village and Rose Bay Beach which are easily accessed via New South Head Road. Other urban and cultural attractions, business and services in the wider context including Kings Cross/Darlinghurst Area, Sydney CBD are accessible via frequent public transport services.

3.3 PRELIMINARY ADVICE

Figure 18. Aerial showing the site in context and pedestrian crossings.

TRAFFIC AND TRANSPORT

According to the report prepared by TTP, the site is very well serviced by rail and bus services and employment, entertainment and other retail facilities in the City and Bondi Junction etc are easily accessed by the public transport services. The assessment of the potential traffic and parking implications of an envisaged residential apartment development at Edgecliff concludes the following:

- The proposed will provide suitable and adequate parking on-site.
- The proposed will not present any unsatisfactory traffic capacity, safety or environmental related implications.

ARBORIST ADVICE

Advanced Treescape Consulting have prepared an Arboricultural Assessment to inform the proposal. The report discusses 15 existing trees and notes the following:

- One tree is nominated as having High signification rating which is the Jacaranda tree located at the front of the site. The tree is nominated as having High Priority for Retention value (Sustainable Retention Index Value (SRIV)) and is recommended for retention.
- 3 trees are nominated as having Medium Priority for Retention value (SRIV) including two palm trees nominated for re-location.
- 9 trees are nominated as having a Low SRIV rating and one tree has a Priority for Removal.

3.4 DEVELOPMENTS HEIGHTS

GMU have review existing development heights in the area.

Existing development along New South Head Road offers a diverse mix of architectural styles and building heights ranging from 2 to approximately 30+ storeys. GMU's study of the existing building heights within the centre and its fringe areas has the following character:

- 3-5 storey residential and mixed use developments within Edgecliff Centre along New South Head Road from New Beach Road to Ocean Avenue.
- Existing higher density developments at the eastern and western arrival points of the Edgecliff centre are 12-14 storeys.
- The existing 7-storey building of the Edgecliff Centre and a 32-storey residential building located at No. 1-3 Darling Point Road.
- Development at No. 6-10 & No. 51 Darling Point Road are 7-13 storeys.
- Recent DA approval at Nos. 80-84 & 90 New South Head Road will deliver 7 storeys.
- No. 311 New South Head Road comprises an 8-storey residential flat building (Edgecliff Towers).
- No. 240 New South Head Road a recently developed contemporary residential flat building with a flat roof presents as part 5, part 6 storeys.
- A Planning Proposal was recently been approved to increase the height and FSR controls at Nos. 80-84 and 90 New South Head Road to 23.5m (7 storeys).
- Recently, a Planning Proposal was approved for the site located at No. 374 and 376-382 enabling a 6 storey development.

Our analysis shows an inconsistent height character for the Edgecliff and Double Bay area along New South Head Road, due to the low-scale older stock and heritage items adjacent to newer higher density development.

GMU have also examined the local development pattern and we note that larger lots are generally found in Edgecliff whereas Double Bay includes a 'fine-grain' subdivision pattern with smaller lot frontages.

Overall, a street wall height of 3-8 storeys exists along New South Head Road. The concentration of larger scale developments occurs at the arrival points, on corner sites and around the station (shown in Figure 19). Furthermore, some instances of existing isolated large towers are located in close proximity to the subject site. These tend to dominate the streetscape profile unrelated in their form to the setting and character of the centre.

Figure 19. Existing and approved development heights.

Key

- Mixed Use Centre
- Subject site
- 1-2 storeys
- 3-4 storeys
- 5-6 storeys
- 7-8 storeys
- 9-10 storeys
- 10+ storeys

No. 1-3 Darling Point Road viewed from New South Head Road.

No. 272 New South Head Road viewed from the corner of Henrietta Street.

Indicative design (PP) for No. 3743-382 New South Head Road (Eeles Trelease).

No. 100 New South Head Road view looking east.

No. 286 New South Head Road view looking north (Google Streetview).

No. 311 New South Head Road.

3.5 THE SITE AND ITS IMMEDIATE CONTEXT

The main characteristics of the subject site and its immediate surrounding context are summarised below:

- The subject site is located approximately 2km east of the Sydney CBD. It is located in the New South Head Road Corridor, within the Woollahra Local Government Area (LGA).
- The subject site is located on the northern side of the road and is known as No. 252-254 New South Head Road, Double Bay.
- The lot is described as SP11702 and the site area is nominated as 934.9 sqm (Survey).
- The site is zoned R3 Medium Density Residential under the Woollahra LEP 2014. The site is nominated as part of the Double Bay Residential Precinct under Woollahra DCP 2015.
- The subject site is located at the Edgecliff Town Centre area within approximately 200m of the Edgecliff Train Station and the Edgecliff Centre.
- Double Bay Village with an outdoor dining strip is located within 300m.
- The site is located to the east of the ridge line, where New South Head Road descends towards the lower lying basin and Double Bay Town Centre. Along this particular part of New South Head Road, the sites to the north of the road slope away from the street (north) whereas the lots fronting the southern side of New South Head Road are elevated above street level, ascending towards Edgecliff Road. Retaining walls and exposed rock faces are visible along the southern side of New South Head Road.
- The area includes a number of heritage items, buildings with contributory character and conservation areas but not adjacent to the subject site.
- Diverse vegetation and clusters of mature trees are important character elements of the area, forming 'green gateways' along major roads providing containment to the street and canopy coverage to both the street and pedestrian environment.
- Filtered canopy views to the north are available in selected locations with view corridors to the rear of the lots available between some buildings. District views are available along road corridors and from the public domain.
- The adjacent developments to the east and west of the subject site have windows facing the common side boundaries.
- The street wall scale to this part of New South Head Road varies from 2 to 8 storeys with many buildings providing a continuous form without secondary setbacks above the streetwall.

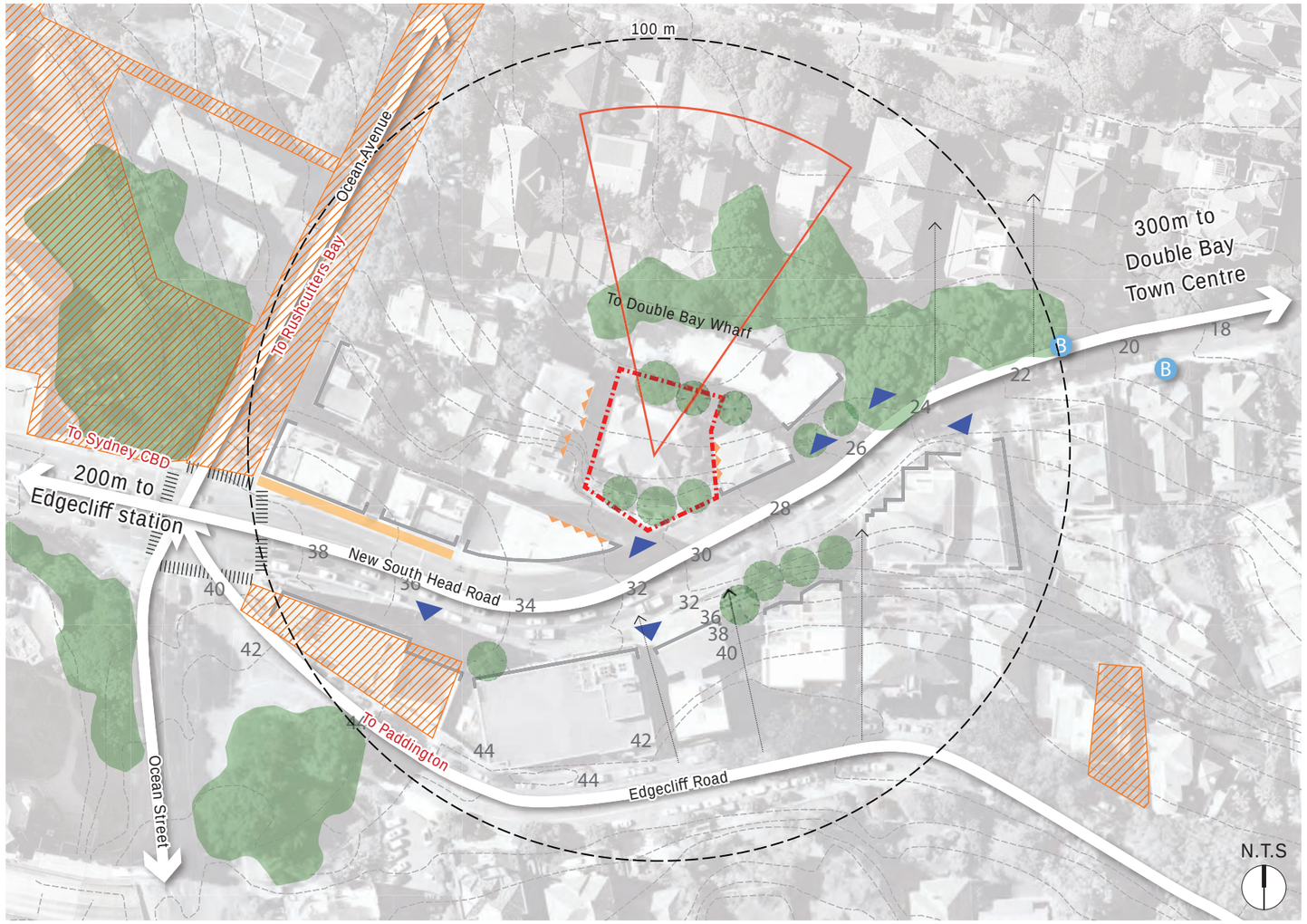


Figure 20. Diagram showing key characteristics and immediate context.

Key

- Subject site
- Bus stop
- Pedestrian crossing
- Heritage item or Conservation Area
- Existing vegetation
- Filtered (canopy) views from the site
- View corridors between buildings
- Windows facing the common boundary
- Active retail/commercial frontage
- Vehicular access points
- Building frontage
- Topography

View looking northeast across the subject site from the driveway of No. 250.

View looking east along the southern boundary (front) of the subject site.

View along the western boundary where windows in the adjoining development (No. 250) are facing the common boundary.

Subject site viewed from New South Head Road (Google Streetview).

View along the western boundary.

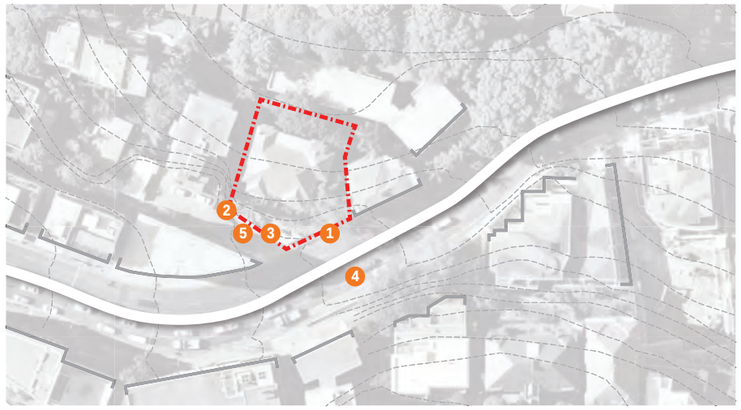


Figure 21. Key view map.

3.6 STREETSCAPE CHARACTER

STREETSCAPE- NORTHERN SIDE OF NEW SOUTH HEAD ROAD

The streetscape character of the northern side of New South Head Road shows varied architectural style and street wall heights.

This particular section of New South Head Road includes a combination of older buildings with more 'fine-grain' character and larger-scale recent developments. Setbacks vary with nil setback predominately provided to the west of the subject site. Front setbacks increase as the road descends towards Double Bay and the lower lying basin.

There are sporadic groupings of trees along the northern side of the road. Clusters of mature vegetation and large trees provide strong definition. Due to the curvilinear alignment of the road, where some lots are irregular in shape and orientation. For many of these lots, mature trees and landscaped settings provide containment to the street.

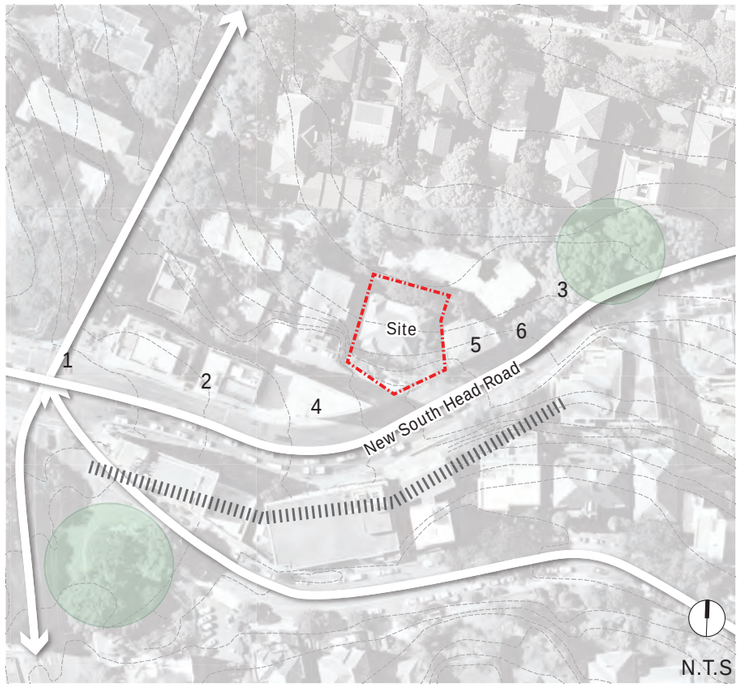


Figure 22. View plan diagram.

1- The Continuous built forms provide nil setback to the street, and provide strong definition to the street and pedestrian environment (Google Streetview).

2- The northern side of New South Head Road includes an eclectic mix of architectural styles and streetwall heights.

3- No. 260 to the east of the site is setback from the street alignment and is screened by a row of trees. The slope of the site is partially visible from the street.

4- The recent 6-storey development to the west of the site provides a contemporary continuous built form where the road changes alignment.

5- No. 256 to the east of the site presents 3 storeys and nil setback to the front boundary.

6- Substantial trees are located within the front setback of No. 264. The trees form an important part of the streetscape character.

STREETSCAPE - SOUTHERN SIDE OF NEW SOUTH HEAD ROAD

A study of the streetscape character of the southern side of New South Head Road shows variation in streetwall heights and setbacks, similar to the northern side. However, due to the sloping topography, developments fronting Edgecliff Road to the rear are visible and form part of a layered streetscape profile when viewed from New South Head Road.

1- The lots at Nos. 297 and 311 are located at similar levels as the street.

2- To the east of No. 311, the terrain ascends to the south and front setbacks are increased.

3- Traditional residential flat developments are visible to the south.

4- Contemporary residential flat developments to the southeast of the subject site. The recent developments reflect the staggered site orientation and fine grain subdivision pattern found in Double Bay.

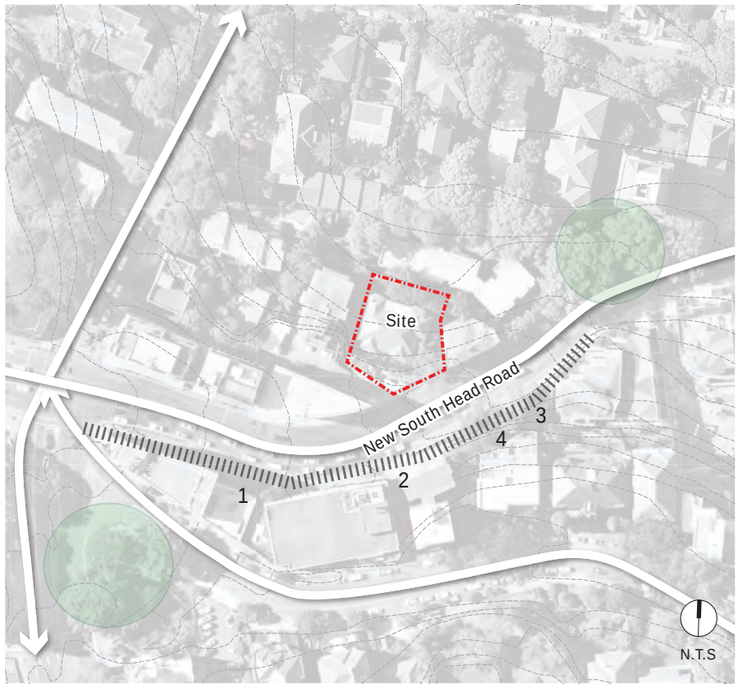


Figure 23. View plan diagram.

3.7 SITE CONSTRAINTS

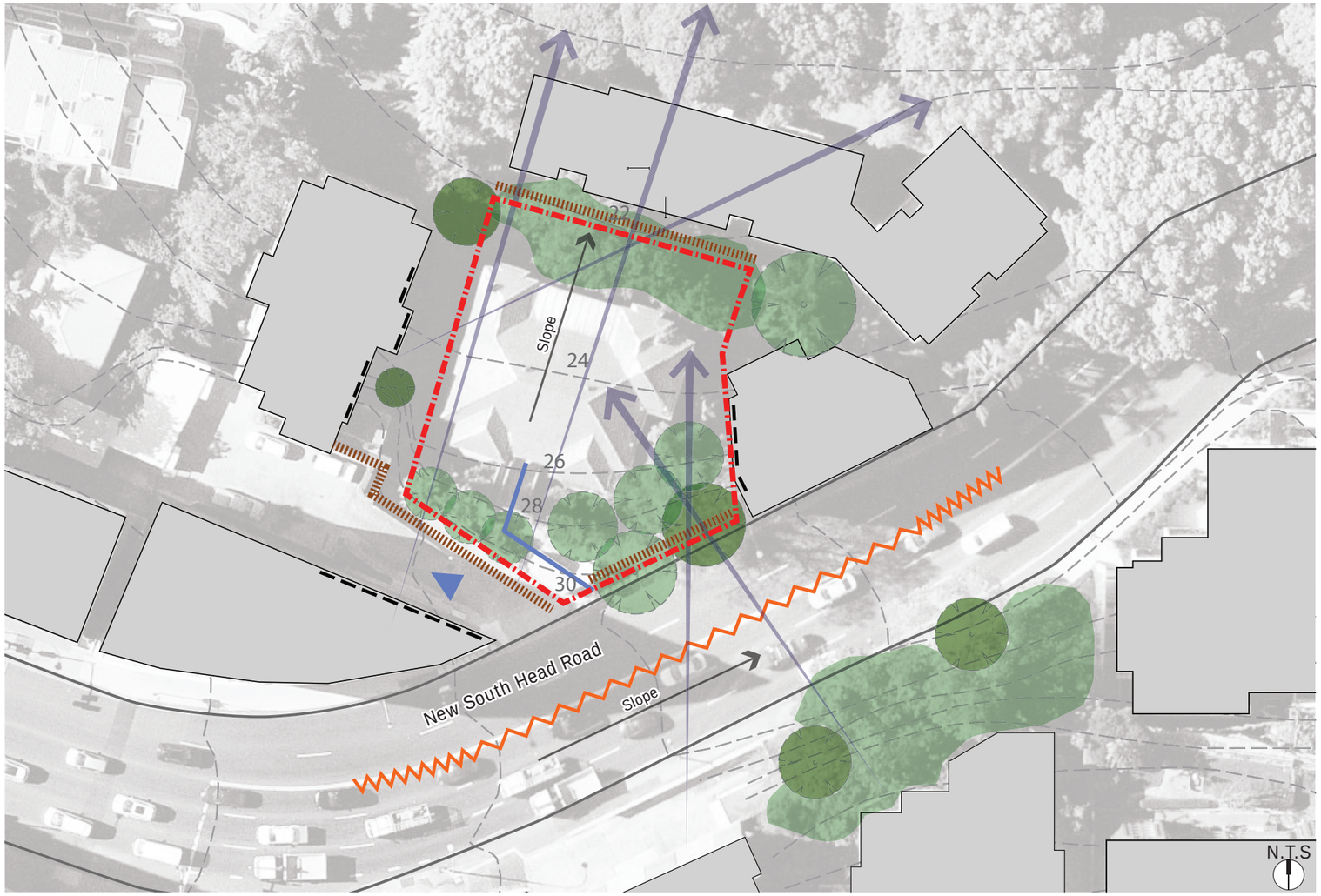


Figure 24. Diagram showing key site constraints.

Key

- Subject site
- Existing Pedestrian access
- Vehicular access
- Traffic movements
- Existing buildings
- Existing vegetation
- Residential windows facing common boundaries
- Edge condition/retaining wall
- Existing slope
- Potential cross site views
- Topography

GMU has investigated the main constraints of the subject site. The key constraints include the following:

- The site fronts New South Head Road which is an arterial road with significant traffic volumes.
- The lot is irregular in shape with limited street frontage.
- The site includes substantial landscaping and a number of mature trees are also located on adjoining sites, close to the boundary.
- The site can currently be accessed via two separate pedestrian entrances off New South Head Road but no vehicle parking is available on site.
- The site slopes to the north and is located below street level.
- Compromised outcomes to site edges due to the significant slope.
- The vehicular entry to the adjoining site is located along the southwestern site edge.
- The adjoining residential properties provide insufficient setbacks and habitable windows overlooking side boundaries.
- Potential view impacts to residential developments to the east, south and west.
- Potential for additional overshadowing to the southern and eastern neighbours.

3.8 SITE OPPORTUNITIES

Figure 25. Diagram showing key site opportunities.

Key

- Subject site
- Significant vegetation
- Existing buildings
- Improved street address
- Filtered canopy views
- Existing slope
- 'Stepped' built form in response to the topography
- Improved cross boundary interface
- Terrain

GMU consider that the site has the ability to:

- Enhance to the unique character of the streetscape, responding to the location at the edge of the Edgecliff Town Centre area.
- Respond to the unique site characteristics and the sloping terrain, to provide a stepped built form consistent with the outcomes sought by the DCP.
- Provide improved outcomes to site edges and high-quality landscape outcomes to improve on-site amenity and amenity to adjoining sites.
- Provide appropriate scale fronting New South Head Road, responding to the scale of adjoining properties.
- Provide a vegetated front setback, retaining high-value trees where possible to compliment the existing streetscape character.
- Provide side setbacks to maintain amenity to adjacent development.
- Sculpt built form to minimise overshadowing impacts to adjoining properties. Carefully consider the configuration of building footprints to minimise adverse impacts and maximise landscaping opportunities.
- Test view impacts and sculpt built form to minimise view impacts to adjoining residential properties.
- Provide access to public transport services in proximity to the site. We note parking provisions are maximum controls and ample bicycle parking is provided as part of the indicative scheme.
- Provide design excellence and high-quality outcomes to compliment the character of the area.
- Provide a unique lifestyle setting close to public transport, community facilities, the shopping village and open space amenity.

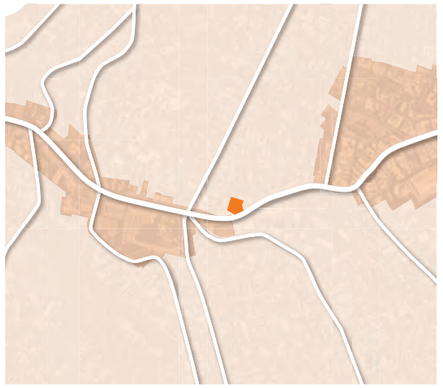
3.9 SECTION CONCLUSION

Our analysis of the local context and streetscape suggests that the subject site has capacity to deliver improved development outcomes to meet the outcomes sought by the local strategic plans and the DCP.

Development heights and densities are likely to increase in coming years in Edgecliff and Double Bay Local Centres. Both centres have capacity to provide high-quality housing outcomes consistent with key strategic aims. The favourable location of the subject site provides potential to contribute to the future desired character of the area with high quality built form outcomes.

Redevelopment of the site will provide an opportunity to deliver improved streetscape outcomes, improved conditions to site edges, enhanced architectural quality to compliment the character of the area. The site is well-placed for redevelopment into a contemporary residential building close to public transport, village setting and unique open space amenity.

4. BUILT FORM STRATEGY AND DESIGN PRINCIPLES



4.1 HEIGHT STRATEGY

HEIGHT STRATEGY FOR THE SITE

Figure 26 (right) shows the existing taller developments along New South Head Road. Maximum existing heights generally range between 5-7 storeys up to 10-14 storeys with a few scattered taller towers; however, these do not have frontages to New South Head Road.

The subject site is located just to the east of the Edgecliff Town Centre area in a prominent location where the road changes alignment. As discussed in the previous chapter of this report, the heights along the northern side of Edgecliff Road vary and due to the sloping topography and lot geometry, some of the buildings are setback from street alignment or located below the level of the footpath.

GMU's streetscape analysis has concluded, that a 4-6 storey streetwall scale will be appropriate for the subject site to provide harmonious streetscape proportions.

Figure 26. Diagram showing existing or approved building heights in the surrounding area of the subject site.

The diagrams on the following page summarise the findings of GMU's streetscape analysis, demonstrating that a 5 storey streetwall presentation compliments the streetscape profile and the surrounding development pattern.

The proposed scale integrates with the development heights along the northern side of New South Head Road, stepping with the sloping topography to the lower scale built forms further east in Double Bay.

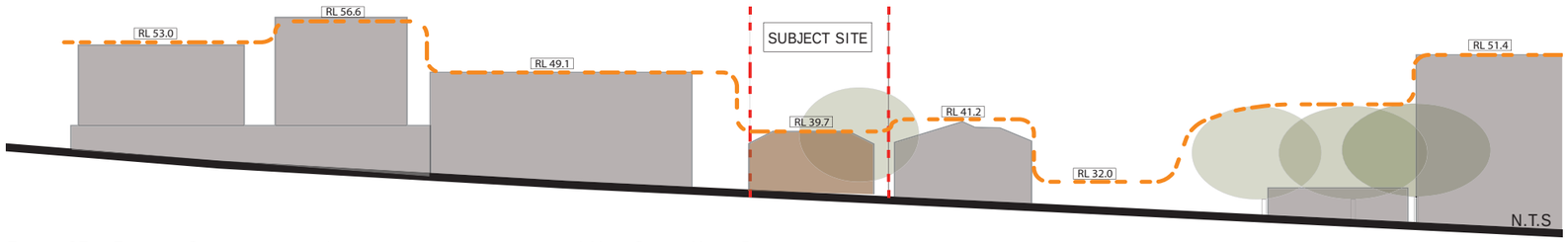


Figure 27. Existing Streetscape profile along the northern side of New South Head Road.

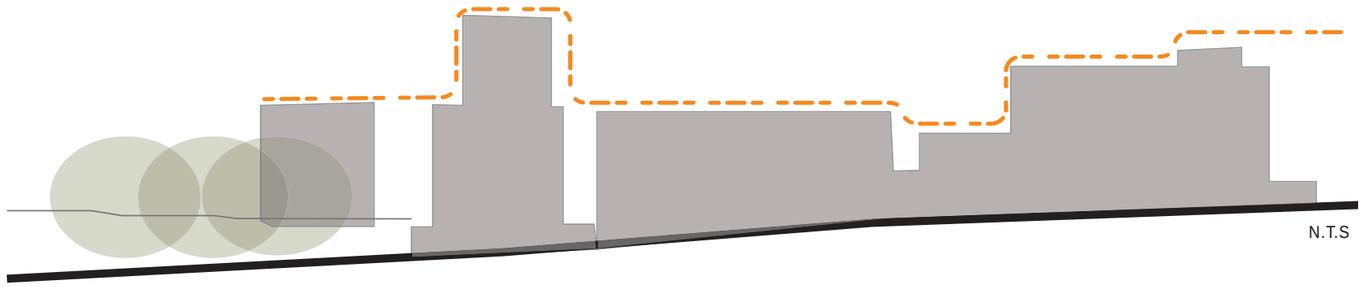
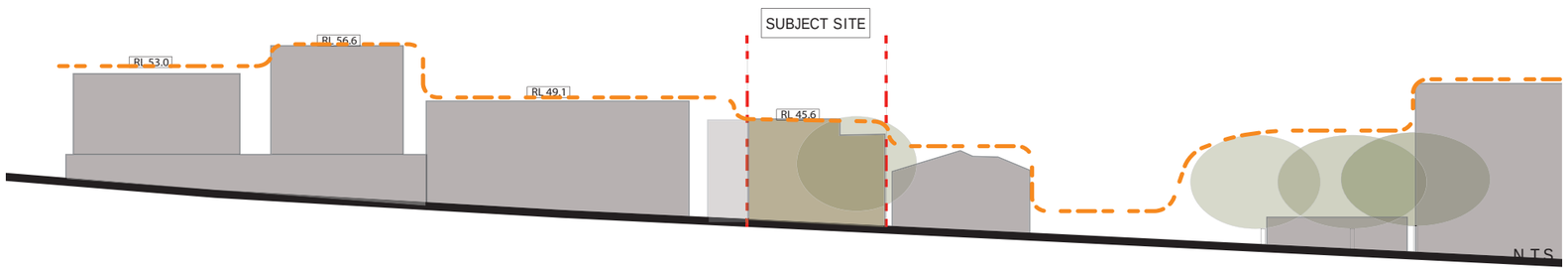


Figure 29. Existing Streetscape profile along the southern side of New South Head Road.



Key

- Subject site
- Existing building (subject site)
- Proposed development
- Road section
- Streetscape profile

Figure 28. Proposed streetscape profile (northern side of New South Head Road).

4.2 THE VISION

In collaboration with the project team, GMU have developed a vision for the future redevelopment of the site.

The subject site will be located at the edge of the area identified in the DCP as Edgecliff Town centre area. The site will take advantage of its central location, good connectivity and unique scenic qualities of the locality and provide diverse high-quality housing offerings, in accordance with state and local strategic aims.

GMU's analysis of the built form context, the constraints and opportunities applicable to the site as well as the existing and potential future streetscape profile suggests that increased scale and density on the subject site will provide a more balanced built form outcome for this part of New South Head Road, adjoining the retail strip and Edgecliff Town Centre where the evolving skyline or profile will gradually cascade with the topography and the slope of the road.

The redevelopment of the site would create harmonious streetscape proportions, transitioning to the taller forms within Edgecliff Town Centre area in response to the existing development pattern and the sloping terrain along New South Head Road.

The proposal will minimise impacts to neighbouring properties due to the aspect and the natural topography of the site which provide opportunity to accommodate built form to the northern part of the site. In the context of recently approved developments, a carefully considered design will elevate the visual qualities of the site and the streetscape, providing high-amenity residential dwellings while taking advantage of the central location close to Sydney's CBD, within walking distance of the Edgecliff Centre, high-frequency public transport, schools, local shops and the unique open space amenity of Double Bay.

Figure 30. Vision images and hand sketches prepared by Antoniades Architects.

4.3 URBAN DESIGN PRINCIPLES

Based on our analysis to date, the following are 'place-based' urban design principles for the site, which are informed by the constraints and opportunities applicable to the site and are intended to guide future development enabling good design outcomes. They aim to:

- Provide a contemporary high-quality residential development to elevate the visual qualities of the site and surrounding streetscape.
- Improve the streetscape proportions by transitioning to the Edgecliff Town Centre area to the west, in response to the existing terrain.
- Respond to the existing street wall height and the proportions of the neighbouring developments to reinforce the continuity of the streetscape.
- Respond to the sloping topography to minimise impacts to adjoining residential properties.
- Provide improved responses to site edges, improving the interface to the neighbouring lots to the east and west.
- Compliment the scale and proportions of the streetscape along New South Head Road.
- Retain significant trees including the prominent Jacaranda to the southern part of the site.
- Compliment and enhance the existing landscape character of the site and the area.
- Provide a stepped building form in response to the topography of the site.
- Ensure appropriate built form articulation to minimise perceived visual bulk.
- Provide landscape screening to site edges adjoining residential lots.
- Apply view sharing principles to minimise impacts to existing residential views.
- Provide appropriate building separation to minimise privacy impacts to adjoining residential uses.
- Provide a diverse apartment mix.
- Provide high-amenity apartments to compliment the favourable location of the site.
- Improve the vehicular access and car parking arrangement.

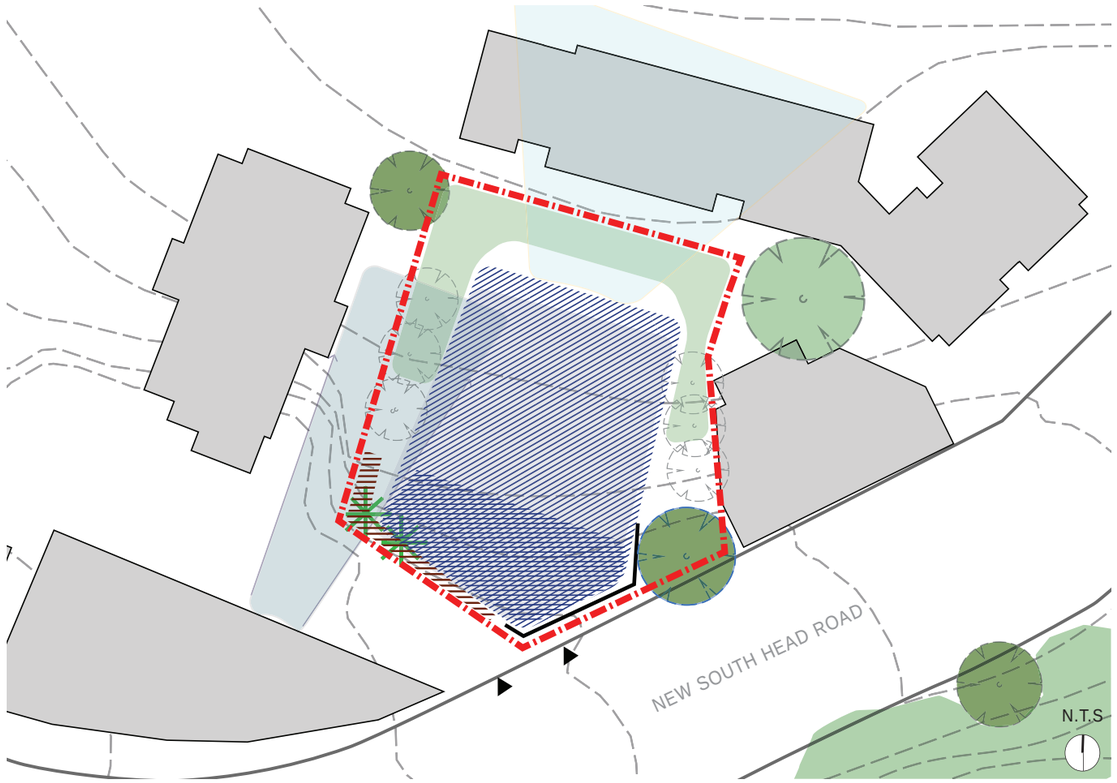


Figure 31. Principles diagram.

Key

- Subject site
- Significant vegetation
- Existing Palm Trees (to be relocated)
- Terrain
- Area for potential future development
- Interface improvements
- Private district views
- Building frontage
- Potential residential views

4.4 SECTION CONCLUSION

GMU consider the proposed scale and built form strategy to be appropriate for the site and the wider streetscape along New South Head Road. Sensitive design solutions will be underpinned by GMU's urban design principles to ensure compatible and robust built form outcomes to compliment the locality.

5. PREFERRED MASTERPLAN

5.1 PREFERRED MASTERPLAN

Figure 32. Diagram showing the preferred masterplan (prepared by Antoniades Architects). N.T.S

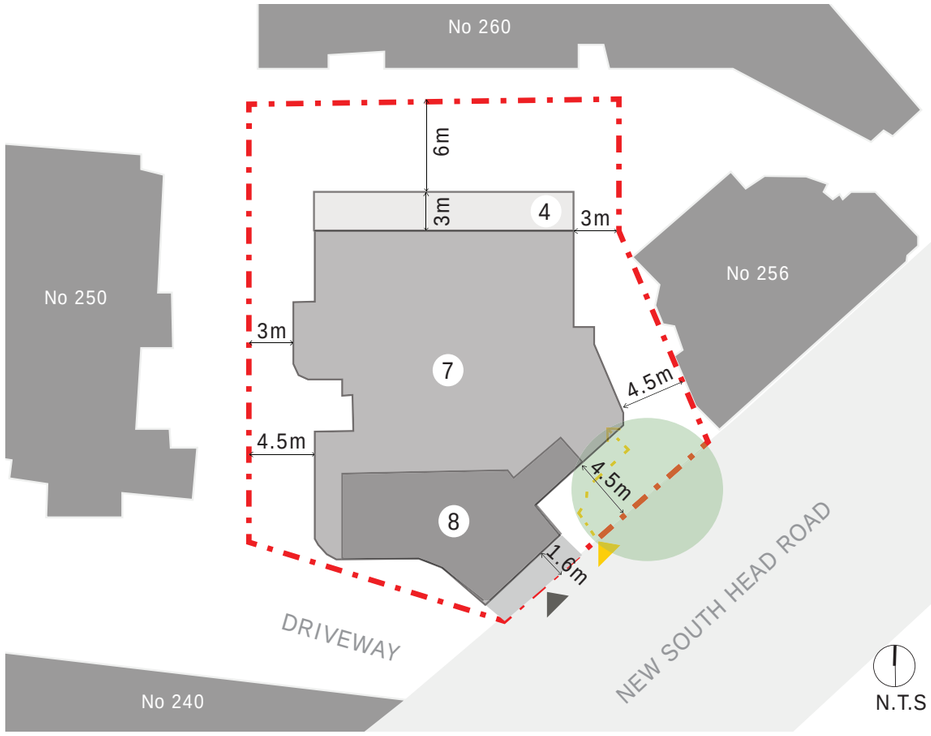


Figure 33. Diagram showing the preferred site configuration.

Key

- Subject site
- Number of storeys
- Pedestrian entry
- Significant vegetation
- Existing buildings
- Vehicular entry

The preferred master plan seeks to provide a built form which is compatible with the streetscape scale. The preferred built form pattern responds to the various alignments created by the irregular site edges improving key cross-boundary relationships as follows:

- Part 5-6 storey streetwall scale in response to adjoining development scales. A small footprint for the upper level to minimise visual bulk and amenity impacts.
- Pedestrian entry sequence to the southern corner of the site, where the existing Jacaranda tree is retained.
- The vehicular entry is located to the adjoining the driveway of the neighbouring site to the south.
- 3m to 4.5m setbacks for improved side boundary conditions with well articulated built forms to accommodate buffer vegetation and improved amenity for future residents as well as for the occupants of the adjoining sites.
- Improved built form response to the sloping terrain ensuring forms 'step' with the topography, taking advantage of the northern aspect and view opportunities.

Preferred built form outcomes include:

- A maximum height up to 8 storeys (maximum RL 46.15 including plant and lift overrun).
- A maximum of 6 storeys to the street (RL 45.90).
- A minimum setback of 1.6m to the southern (front) boundary.
- A minimum setback of 6m to the northern (rear) boundary.
- A minimum setback of 3m to the eastern boundary.
- A minimum setback of 3m to the western boundary.
- Vehicle access from New South Head Road.
- Communal open space on roof top (RL 42.40).

The diagrams below illustrate the preferred massing in context. The proposed increased allowable building height is shown (in blue). Figure 34-36 illustrate how the preferred massing cascades with the sloping topography, proving lesser development scale to New South Head Road.

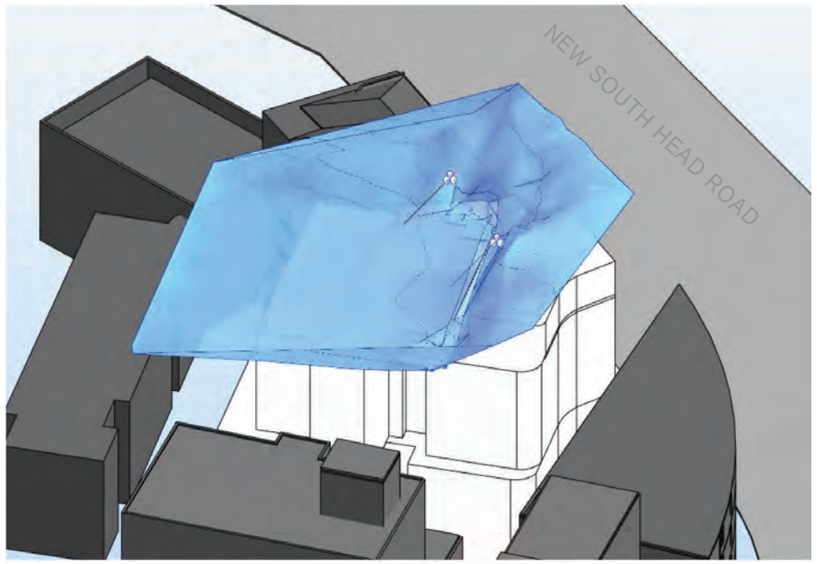


Figure 34. Indicative built form with 22m height plane (Antoniades Architects).

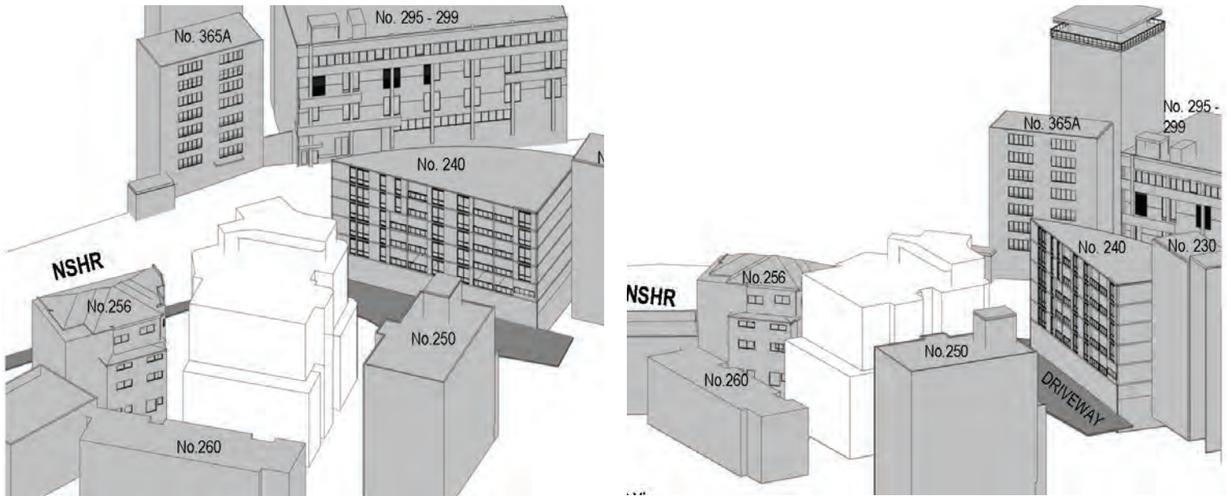


Figure 36. Diagram showing preferred massing which is stepped in response to the sloping site.

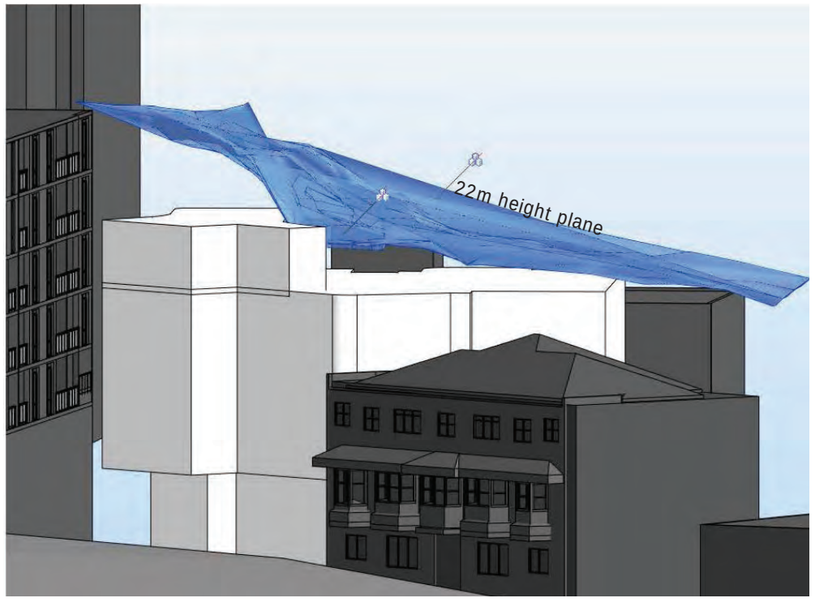


Figure 35. Indicative built form with 22m height plane (Antoniades Architects).

5.2 INDICATIVE LAYOUTS

In order to ensure the preferred building envelope has the capacity to provide reasonable development outcomes and high-quality residential amenity, Antoniades Architects have developed typical layouts as shown below. The layouts are indicative subject to a future Development Application.

Figure 38. Indicative scheme Level 03 Plan (Antoniades Architects).

Level 03 is located at street level (FFL Rl 26.90) and includes the car park entry from New South Head Road. Level 03 also includes the main residential entry and associated lobby areas. 4 residential units are orientated to the north. Privacy impacts and improvements to site edges have been carefully considered.

Figure 39. Indicative scheme Typical Floor Plan Level 05 (Antoniades Architects).

The typical Level 05 plan accommodates 4 north facing units plus one unit orientated towards the street (south). Increased setbacks are provided to the northern and western boundaries to reduce visual bulk and minimise adverse impacts to adjoining sites.

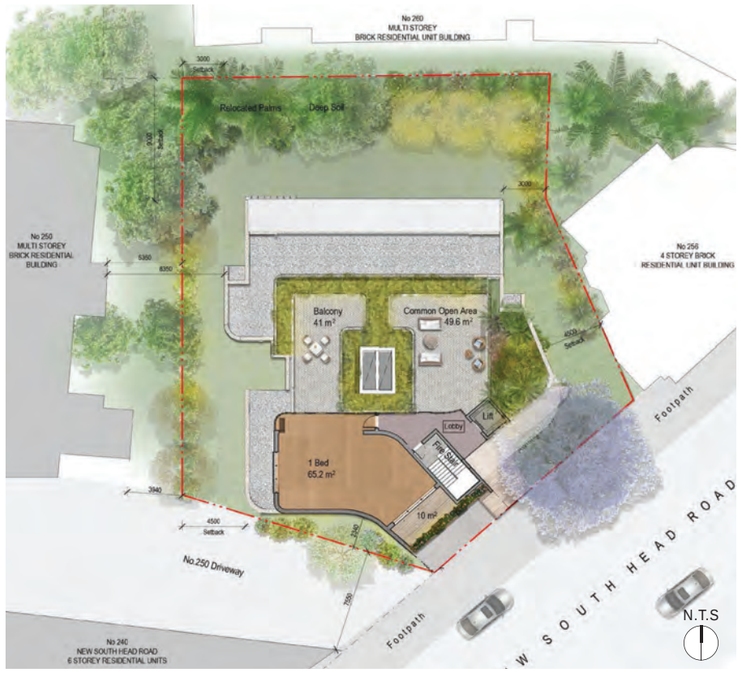


Figure 37. Indicative scheme Roof Level (Antoniades Architects).

The Roof Level accommodates one penthouse apartment within a reduced building footprint. The level also provides access to the north facing communal open space located on the roof.

5.3 SECTIONAL STUDIES

Antoniades Architects have developed typical sections indicating distribution of bulk and scale relative to the sloping site levels. The diagram shows the extent of additional excavation.

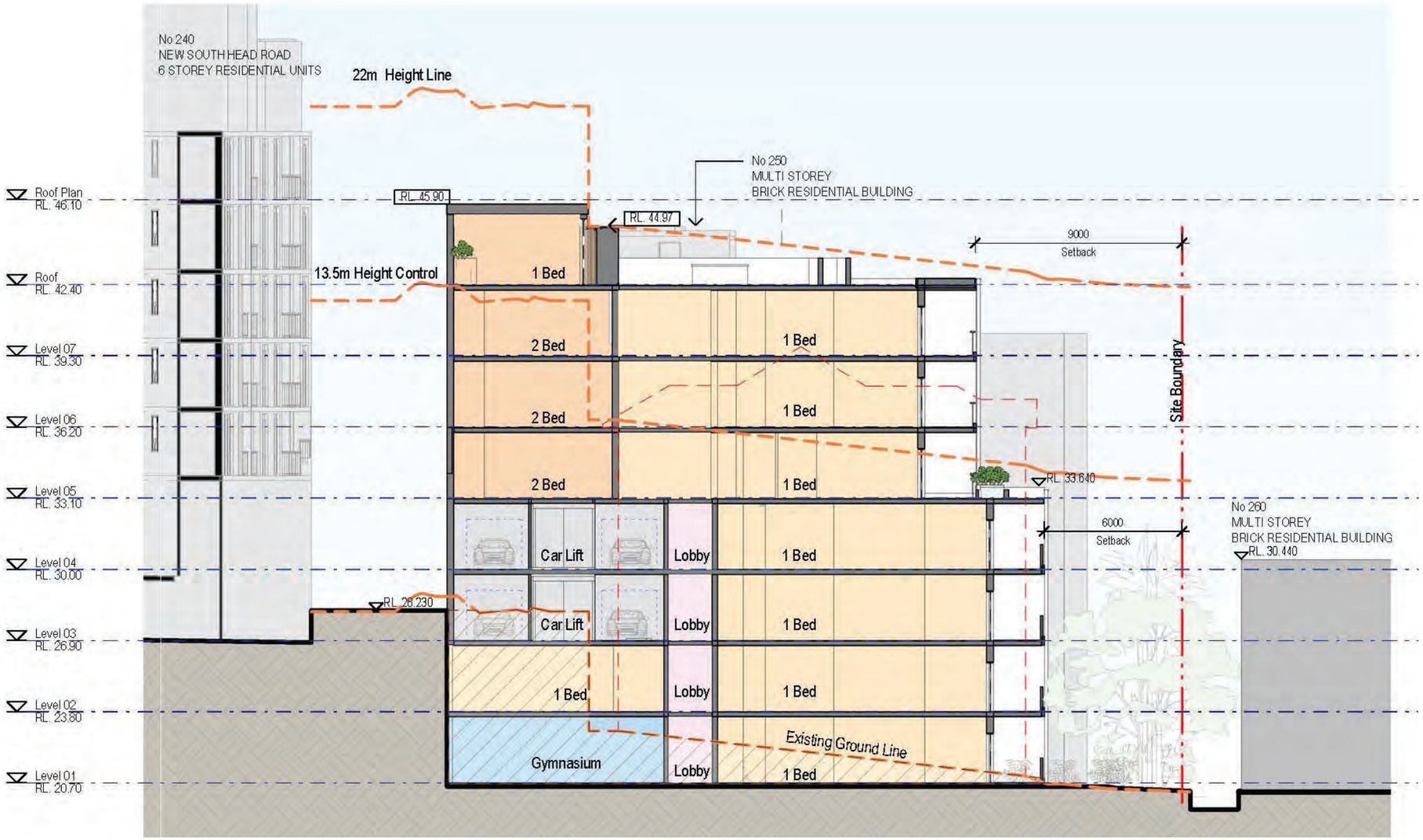


Figure 40. Indicative scheme. Section (by Antoniades Architects).

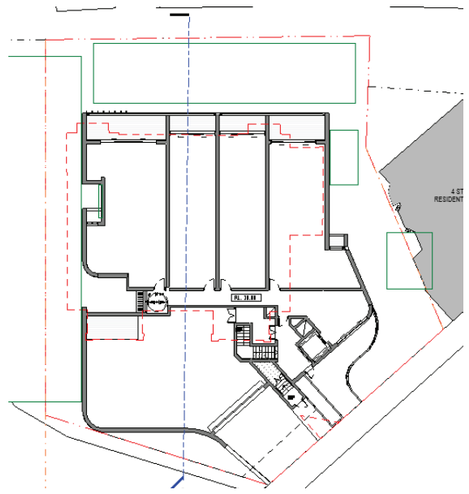


Figure 41. Section plan (by Antoniades Architects).

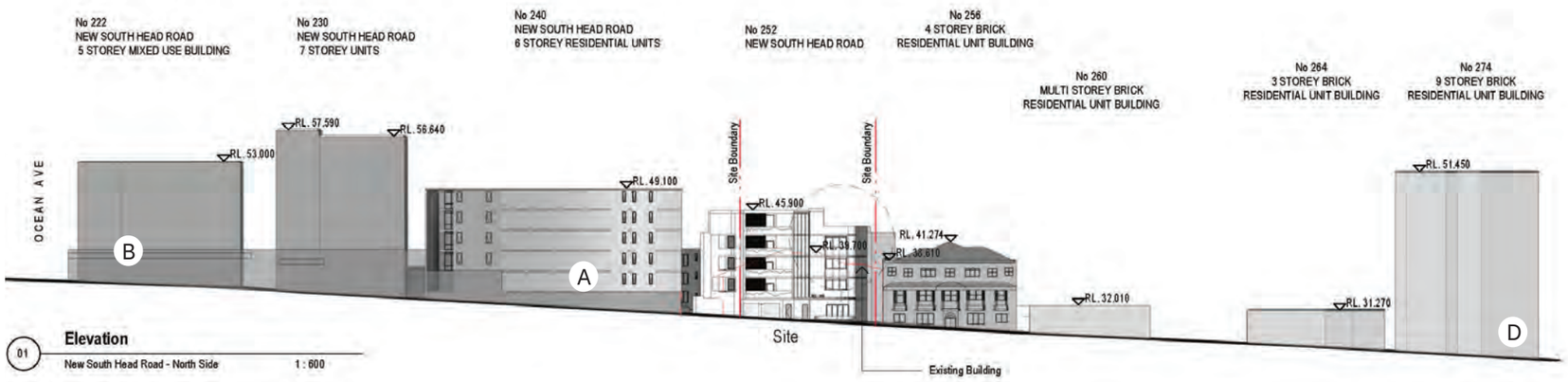
Legend:

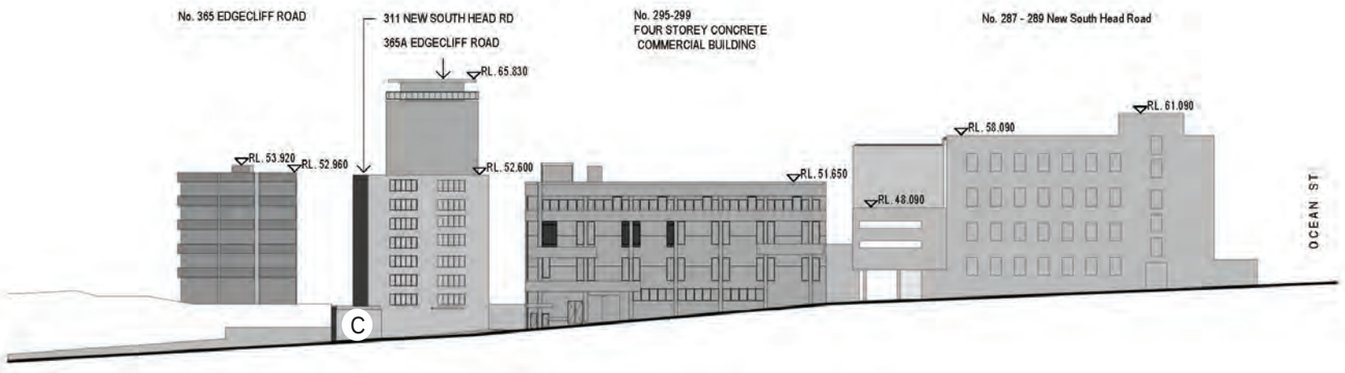
- Property Boundary
- Exsting Building Footprint
- Excavation

5.4 STREETSCAPE ELEVATIONS

A variety of development heights are found along New South Head Road, as demonstrated in GMU's context analysis. Photographs shown on the right illustrate examples of existing mixed use and residential typologies in proximity to the site.

Informed by the height strategy and the preferred massing option, Antionades Architects have developed indicative streetscape elevations (below), demonstrating how future development may respond to the existing development pattern and compliment the streetscape profile.





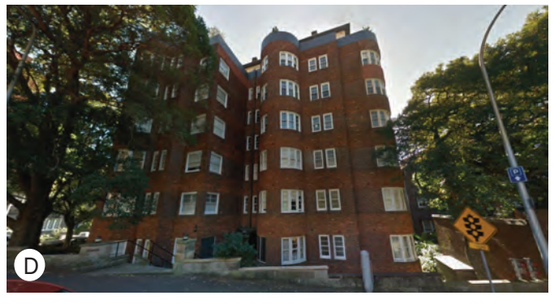


Figure 42. Indicative streetscape elevations (Adapted from elevations by Antoniades Architects).

5.5 LANDSCAPE STRATEGY

A strong landscape and open space concept will compliment the surrounding character and enhance the unique visual amenity of the area. This chapter summarises the landscape concept for the subject site, prepared by Antionades Architects.

Along New South Head Road, clusters of mature trees define nodes along the road sequence, forming a series of characteristic 'green' gateways. Canopy views are available throughout the area from the public and private domains, due to the undulating topography. In response to the existing landscape qualities, the open space should include the following:

- Retention of the existing Jacaranda tree to the southeastern corner of the site. The mature tree provides a visually prominent landscape feature within the streetscape at the entry to the Edgecliff Town Centre area on approach form the east. The tree should be incorporated into future development, marking the pedestrian site entry.
- Relocation of the existing palm trees to a more suitable location at the northeastern site edge.
- Provision of high-quality landscaped communal open space along the northern part of the site. The communal area will include deep soil plantings and trees contributing to the existing mid-block landscape corridor, enhancing the canopy coverage.
- Provision of generous landscape screening and deep soil areas to the northern, eastern and western boundaries to minimise amenity impacts to adjoining properties and improve visual amenity and outlook.
- Provision of landscaped areas (above structure) along the southern boundary to improve the edge condition to the adjoining site and contribute to the 'greening' of the streetscape, when viewed from the pedestrian environment along New South Head Road.
- Green roof at treatments at the upper level, improving the visual quality of the roofscape as well as separating the private open space from the communal open space.
- A roof top communal space contributing to residential amenity, offering district views and high-amenity recreational spaces for gatherings and social interaction.

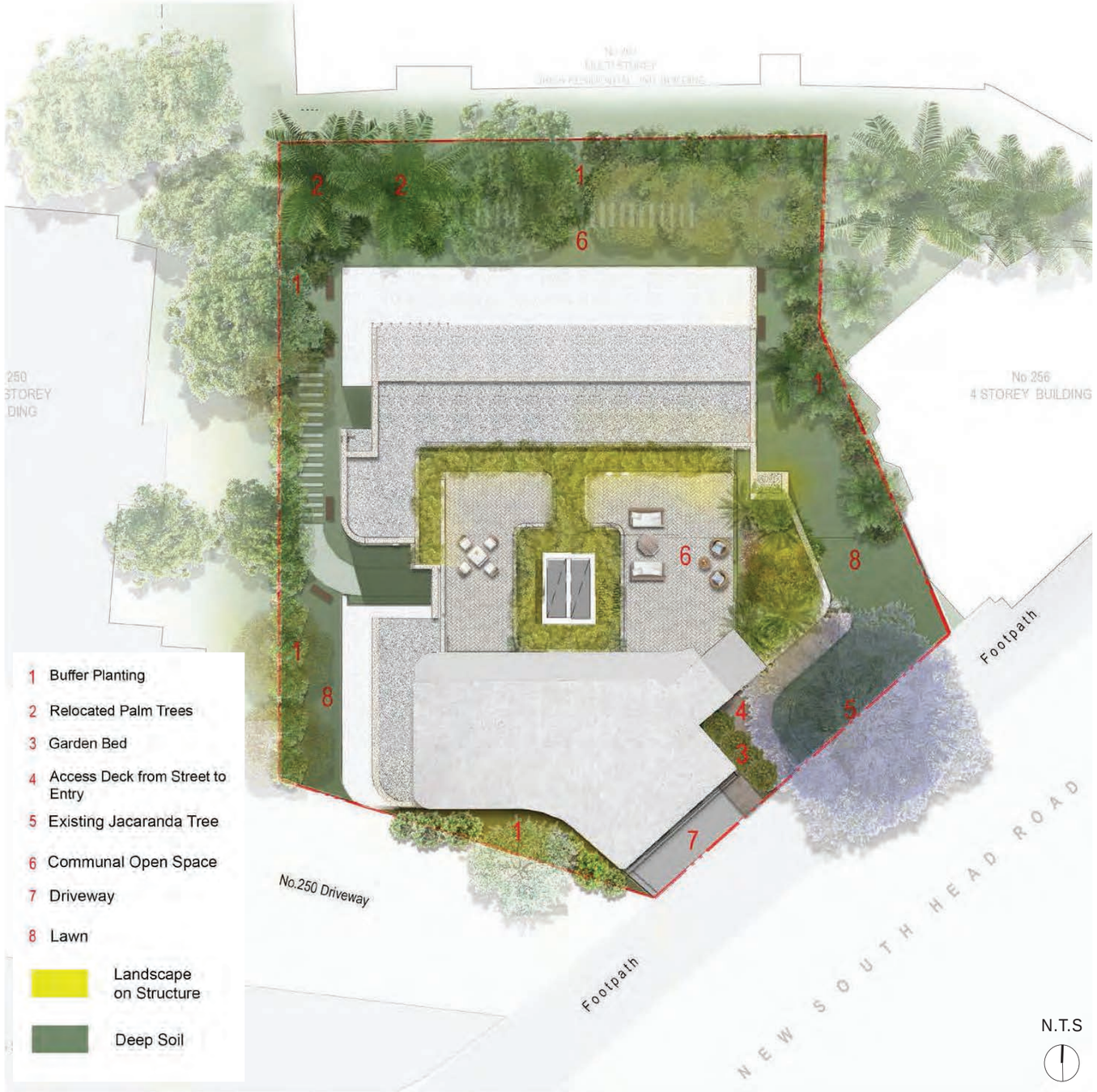


Figure 43. Landscape Concept Plan (Prepared by Antoniades Architects).

5.6 VIEW IMPACT STUDY

As discussed in previous chapters of this report, the scenic quality of Edgecliff and Double Bay is an important part of the character of the locality. Hence, view studies exploring impacts to the public domain and adjoining residential uses have informed the preferred massing option and the indicative layouts.

The sloping terrain and the orientation of the site means both district view and water views are available from the site, in particular towards the north and northeast. This includes water views to Double Bay and Diendagulla Bay. Taller developments at Darling Point are also visible, forming part of the backdrop. Similar views are also available to other residential properties in the area and therefore, potential view impacts to adjoining sites have been carefully analysed as part of the design process to minimise adverse impacts.

Antoniades Architects have modelled the potential impacts using a 3D model of the site and surrounding areas based on survey data, topography information sourced from Six Maps, Google Earth, Cad Mapper and drone photographs.

The comparative analysis prepared by Antoniades Architects concludes public views are largely unaffected by the proposed amendments to the existing controls. However, some impacts would be associated with the proposal to the recently completed property directly to the south at No. 240 New South Head. The potential view impacts to selected habitable views are discussed on the following pages.

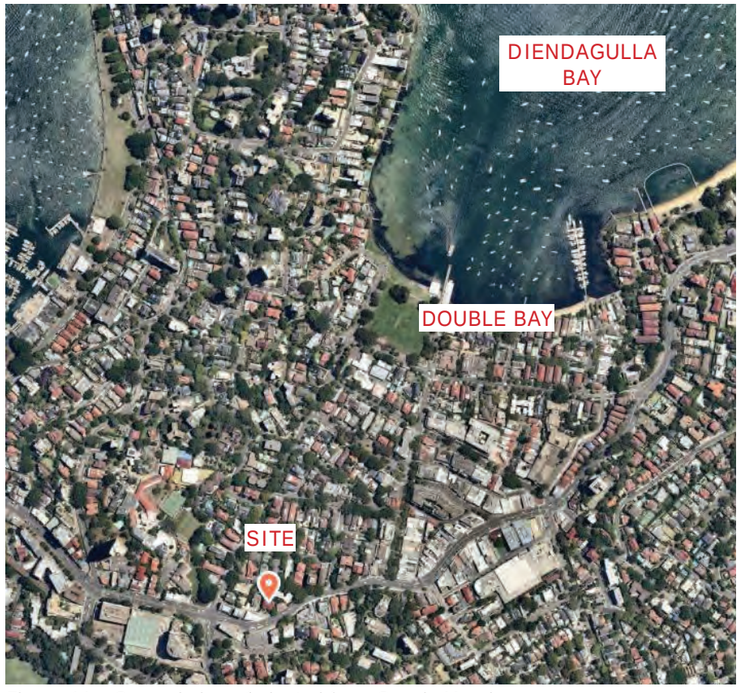


Figure 44. General views (adapted from Google Maps).

The analysis shows that the north facing district views from the lower level units at No. 240 New South Head Road are currently obstructed by the existing development on the subject site and mature vegetation, including the palm trees along the western site boundary.

From mid- and upper-level north facing units at No. 240, distant water views are available. These views are currently partially obscured by the existing mature vegetation but water glimpses and land-water interfaces (Bradley's head) are visible in the distance. These habitable views are considered important and should be substantially retained as part of future redevelopment on the subject site. Potential view affectation should be examined in detail as part of future development applications and view impacts should be assessed against the Tenacity Principles in accordance with court certifiable standards (NSW Land & Environment Court).

Figure 46. Photo showing north facing habitable windows/balconies at No. 240.

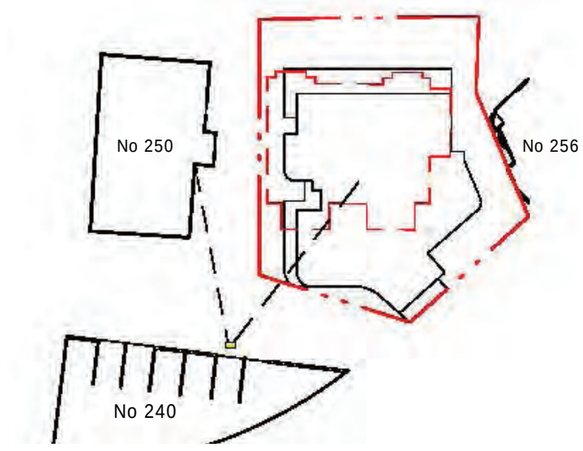


Figure 45. View Analysis location plan (courtesy of Antoniades Architects).

Level 5

Antoniades Architects have modelled the maximum building envelope as well as the indicative scheme. The analysis shows that existing water views as well as land/water interface views are retained. The proposal results in minor loss of foreground canopy views which is considered to be reasonable.

Level 3

The analysis shows existing water glimpses will be largely retained due to the small building footprint of the upper level.

Ground Floor

The analysis shows no water views are available to the unit at Ground Floor Level. The proposal will result in limited sky exposure. However, a vegetated outlook will still be available to the unit at Ground Floor Level. We note the removal of existing vegetation may enable additional water glimpses to some adjoining units.

Figure 47. View location - Level 5 (Antoniades Architects).

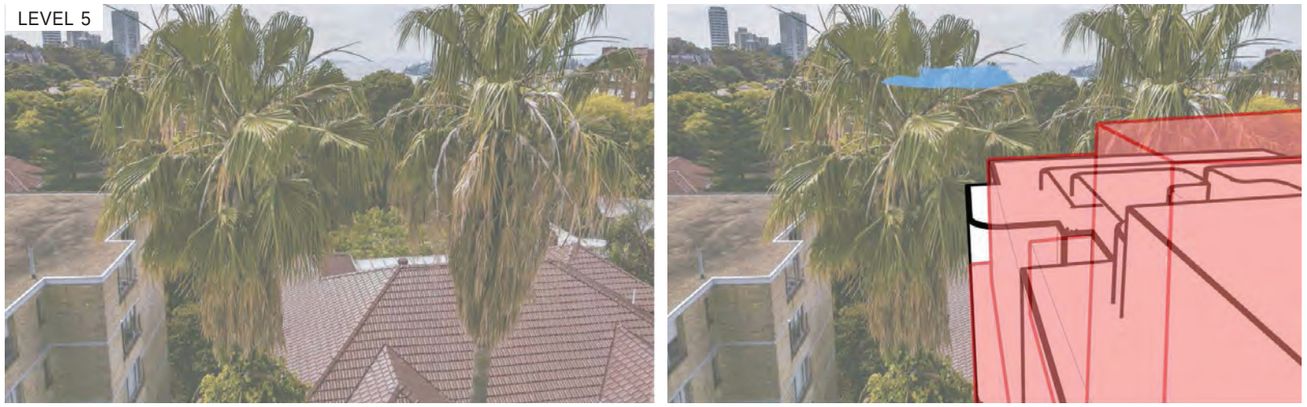


Figure 48. View location - Level 3 (Antoniades Architects).

Figure 49. View location - Ground floor (Antoniades Architects).

Key

- Water glimse
- Indicative scheme
- Maximum building envelope

5.7 SHADOWING IMPACTS

The introduction of a taller form on the subject site will create additional over shadowing beyond the shadows that would be cast by the current height controls of 13.5m.

A shadow analysis has been prepared by the project architects to test the massing at a maximum building height of 22m for the site. The findings of the analysis have informed the preferred massing option for the site. Further detailed studies should be prepared as part of future development applications for the subject site.

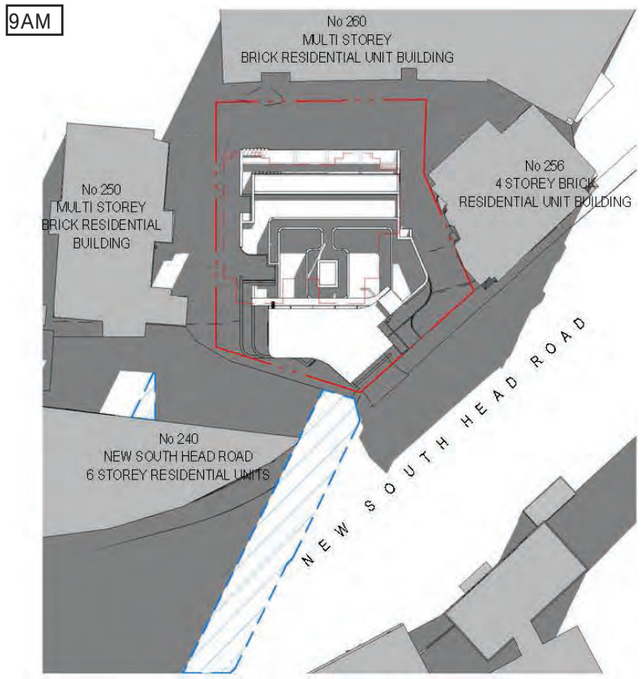


Figure 50. Shadow analysis 9am 21st of June (Antoniades Architects).

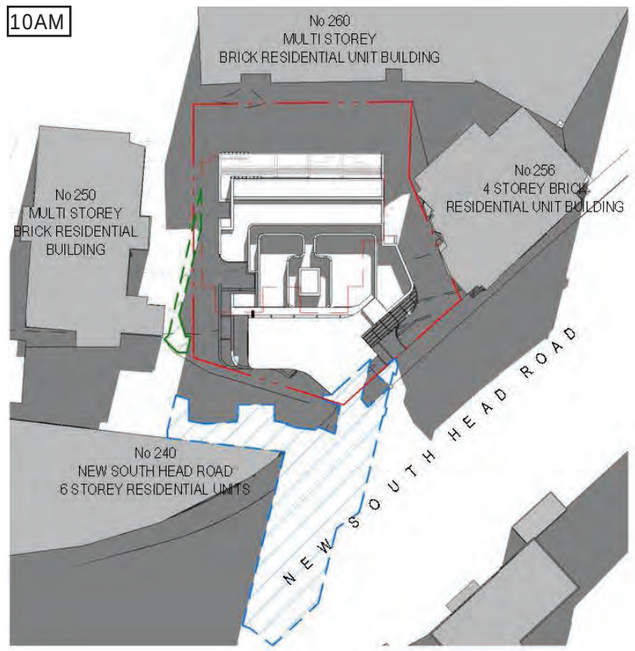


Figure 51. Shadow analysis 10am 21st of June (Antoniades Architects).

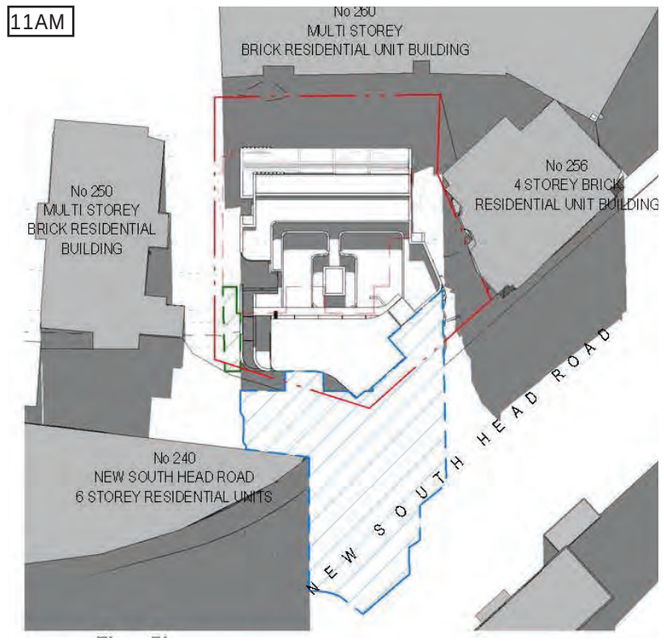


Figure 52. Shadow analysis 11am 21st of June (Antoniades Architects).

Key

- Existing Building
- Existing Shadow
- Proposed Shadow
- Reduced Shadow

The site's side boundaries to the east and west, whilst the front boundary is to the south. Some additional overshadowing will be created resulting in the following:

No. 240 New South Head Road

- Approximately 10am-12pm - Minor additional overshadowing to the residential development to the south at No. 240 New South Head Road however, as demonstrated in the sun-eye diagrams on the following pages, only a few units are impacted during the morning hours.

- 1-3pm - No additional overshadowing impacts occur to the residential development at No. 240 New South Head Road. The additional overshadowing will fall on the road surface and pedestrian areas.

No. 256 New South Head Road

- Approximately 1-3pm - Limited additional overshadowing will also impact the residential development to the east at No. 256 New South Head Road during the afternoon. This is demonstrated in the sun-eye diagrams on the following pages.

Public domain

- 9am-3pm - The majority of additional overshadowing impacts associated with the proposal will impact the road surface and public domain areas.

The shadow study prepared by Antionades Architects shows the additional shadow cast is beyond the existing condition will largely impact the public domain and areas which are already overshadowed by tree canopies. GMU found the level of overshadowing to be reasonable given the dense urban context and the existing compromised boundary relationships.

The following pages include sun-eye diagrams prepared by Antoniades Architects. The diagrams demonstrate solar access and potential overshadowing impacts associated with the indicative scheme.

Key

- Existing Building
- Existing Shadow
- Proposed Shadow
- Reduced Shadow

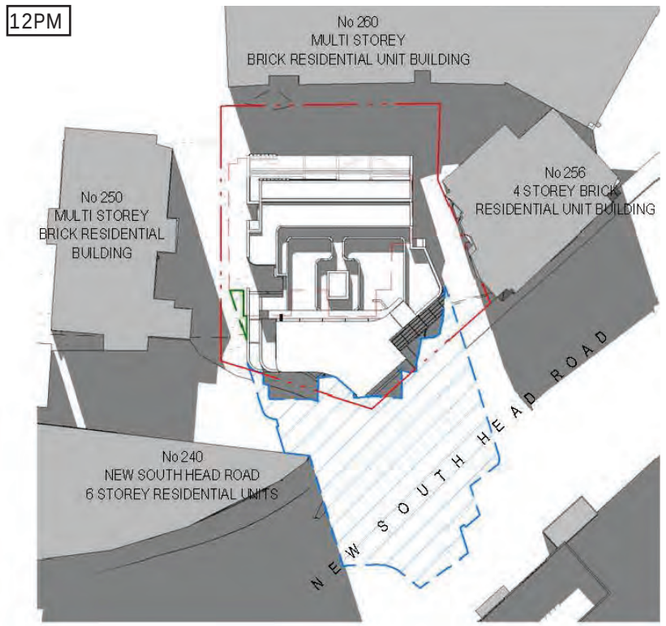


Figure 53. Shadow analysis 12pm 21st of June (Antoniades Architects).

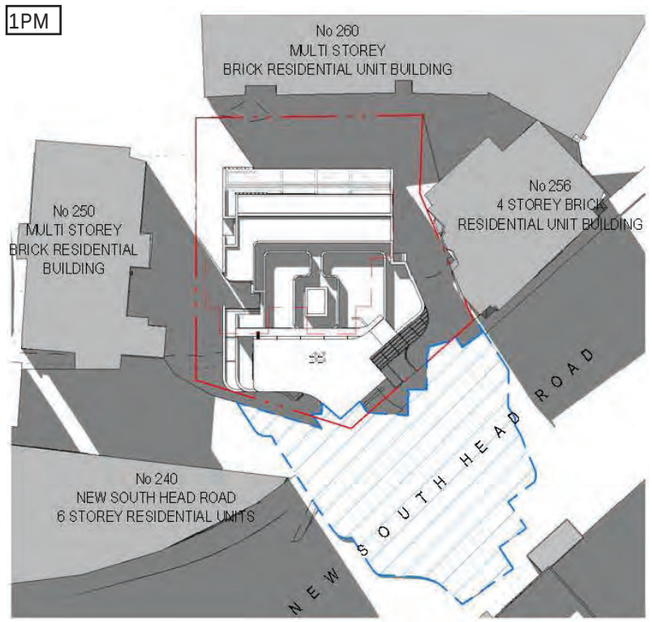


Figure 54. Shadow analysis 1pm 21st of June (Antoniades Architects).

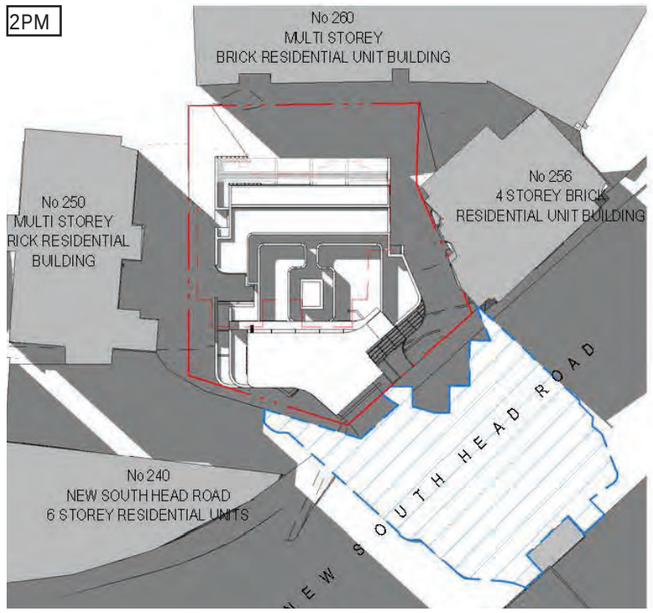


Figure 55. Shadow analysis 2pm 21st of June (Antoniades Architects).

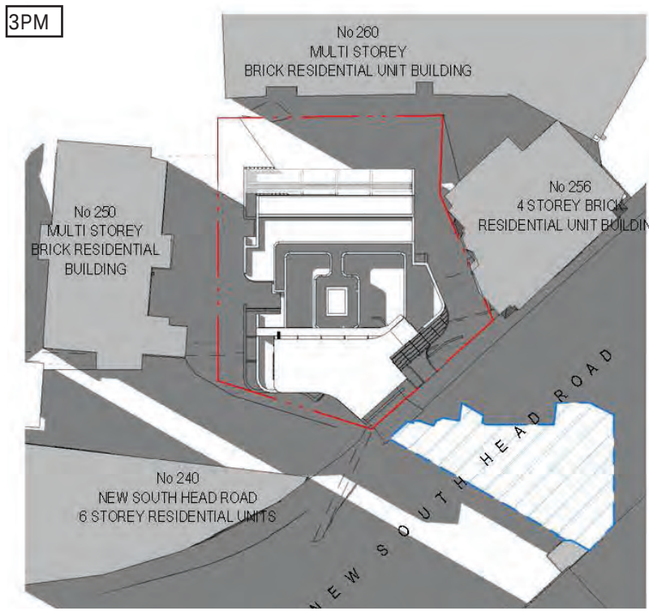


Figure 56. Shadow analysis 3pm 21st of June (Antoniades Architects).

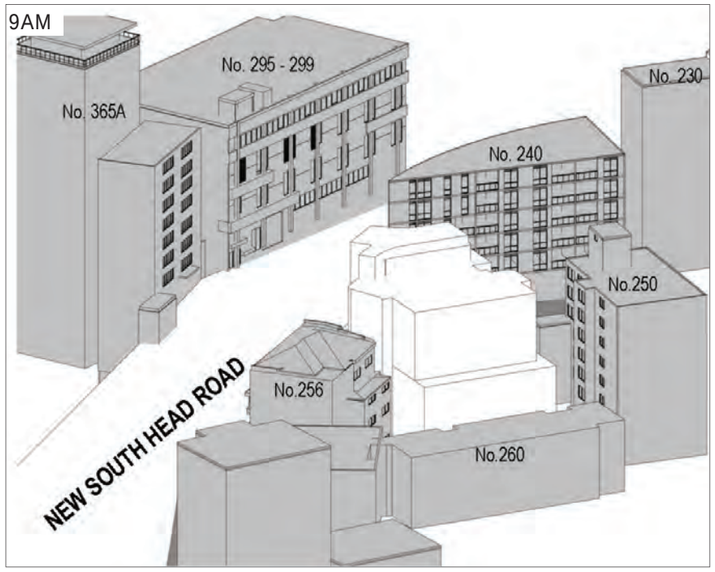


Figure 57. Sun-eye diagram 9am 21st of June (Antoniades Architects).

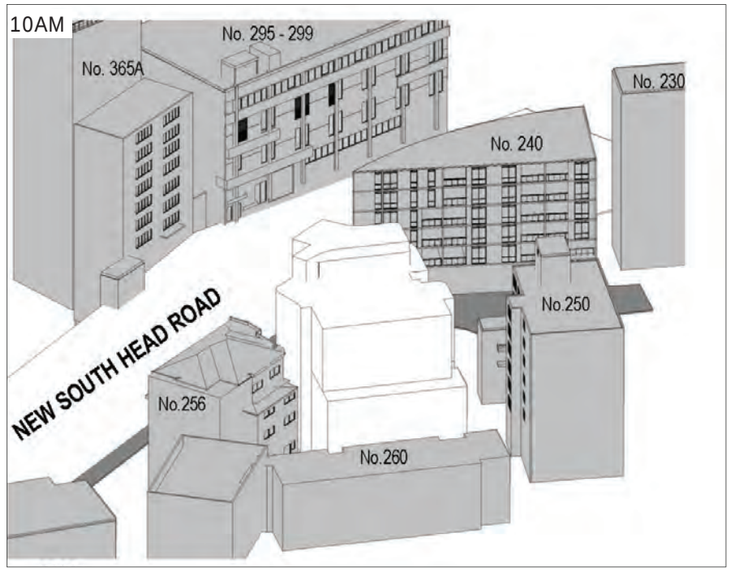


Figure 58. Sun-eye diagram 10am 21st of June (Antoniades Architects).

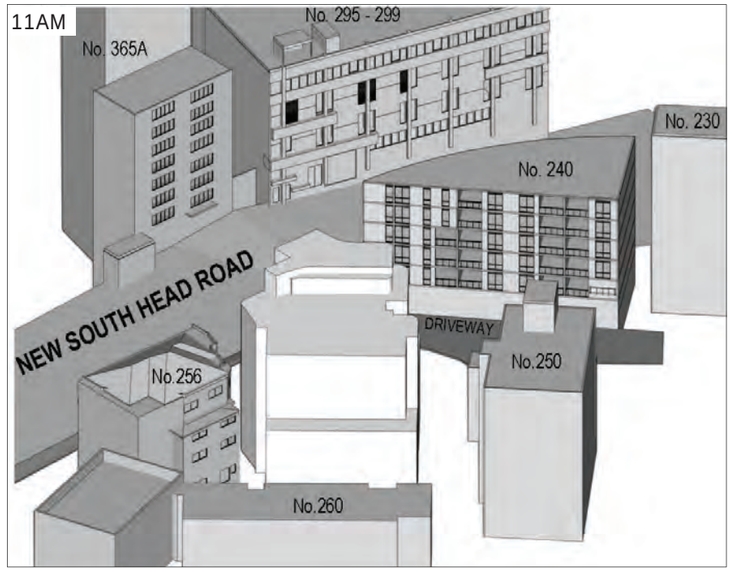


Figure 59. Sun-eye diagram 11am 21st of June (Antoniades Architects).

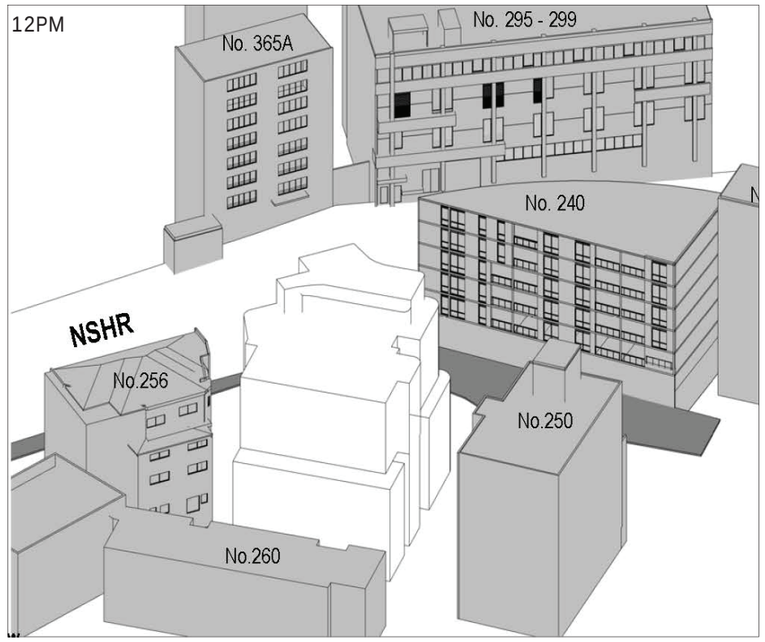


Figure 60. Sun-eye diagram 12pm 21st of June (Antoniades Architects).

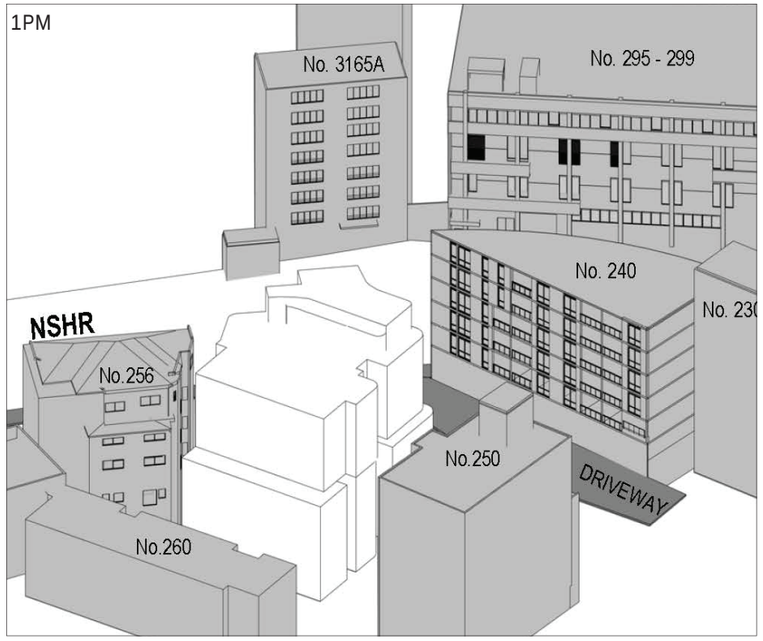


Figure 61. sun-eye diagram 1pm 21st of June (Antoniades Architects).

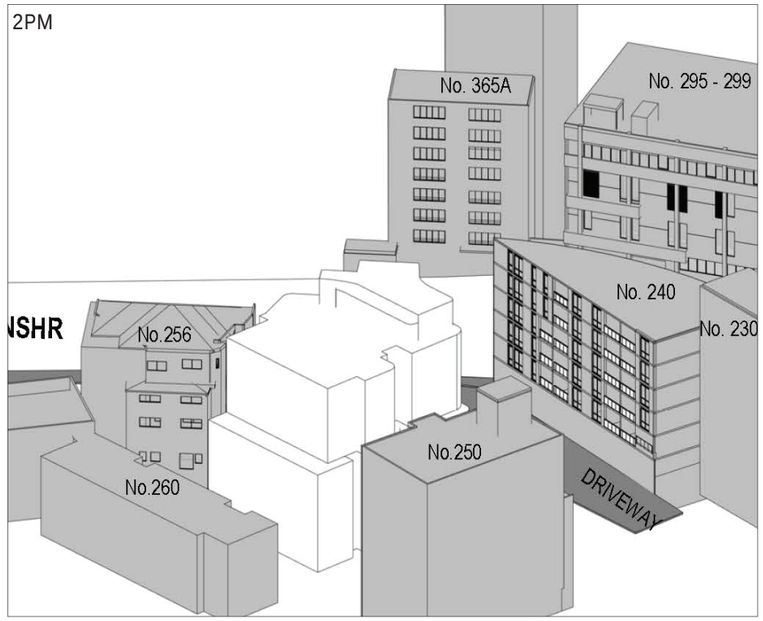


Figure 62. Sun-eye diagram 2pm 21st of June (Antoniades Architects).

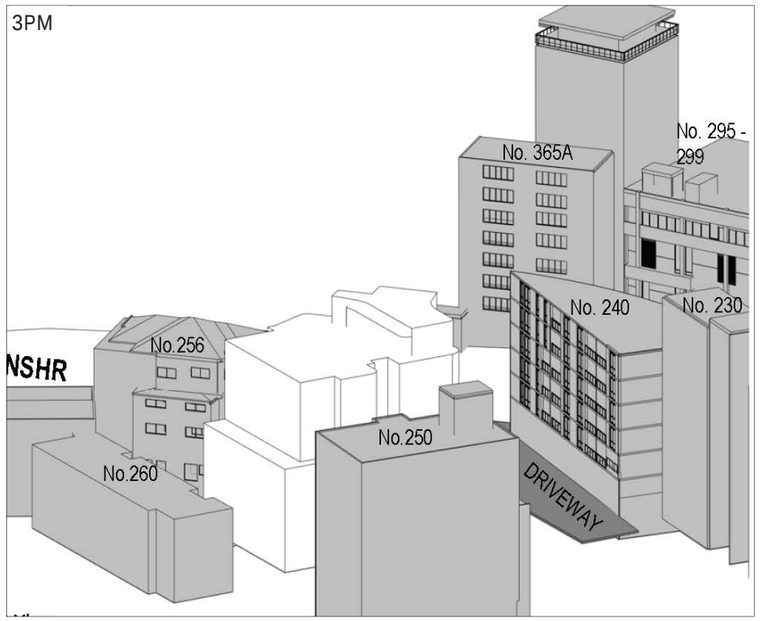


Figure 63. Sun-eye diagram 3pm 21st of June (Antoniades Architects).

5.8 APARTMENT DESIGN GUIDELINES

To ensure good levels of residential amenity are achievable, Antoniades Architects have tested the typical layouts to ensure ADG requirements for solar access and cross ventilation are met. The findings are summarised in the diagrams below. Units meeting the solar access requirement of 2 hours are marked with a sun marker.

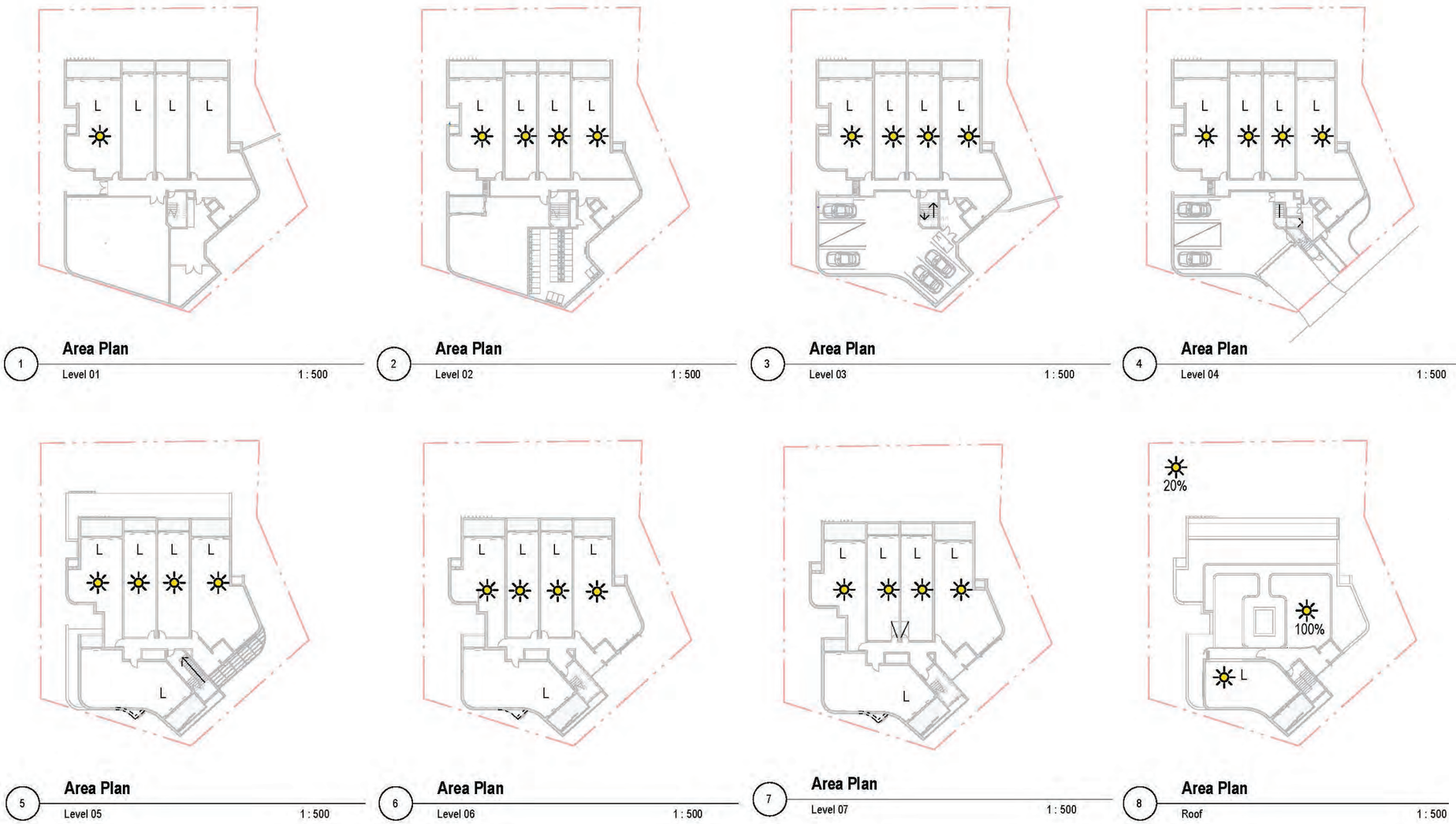


Figure 64. Solar access diagrams (Antoniades Architects).

According to the information provided by Antoniades Architects, 78.8% of units (26 units of 33 units) achieve minimum 2 hours of solar access during mid-winter. This is in accordance with ADG guidelines. Only 2 units receive less than 15mins solar access which equates to 6% of the total number of units.

233.8sqm Communal Open Space is provided which equates to 25% of the total site area. Testing demonstrates that more than 50% of the principal usable communal open space receives 2 hours of solar access during mid-winter. This outcome is appropriate for the site and consistent with ADG guidelines.

Units meeting the ADG cross ventilation requirements are shown in the diagrams below. Units receiving natural cross ventilation are marked with a linear airflow symbol.

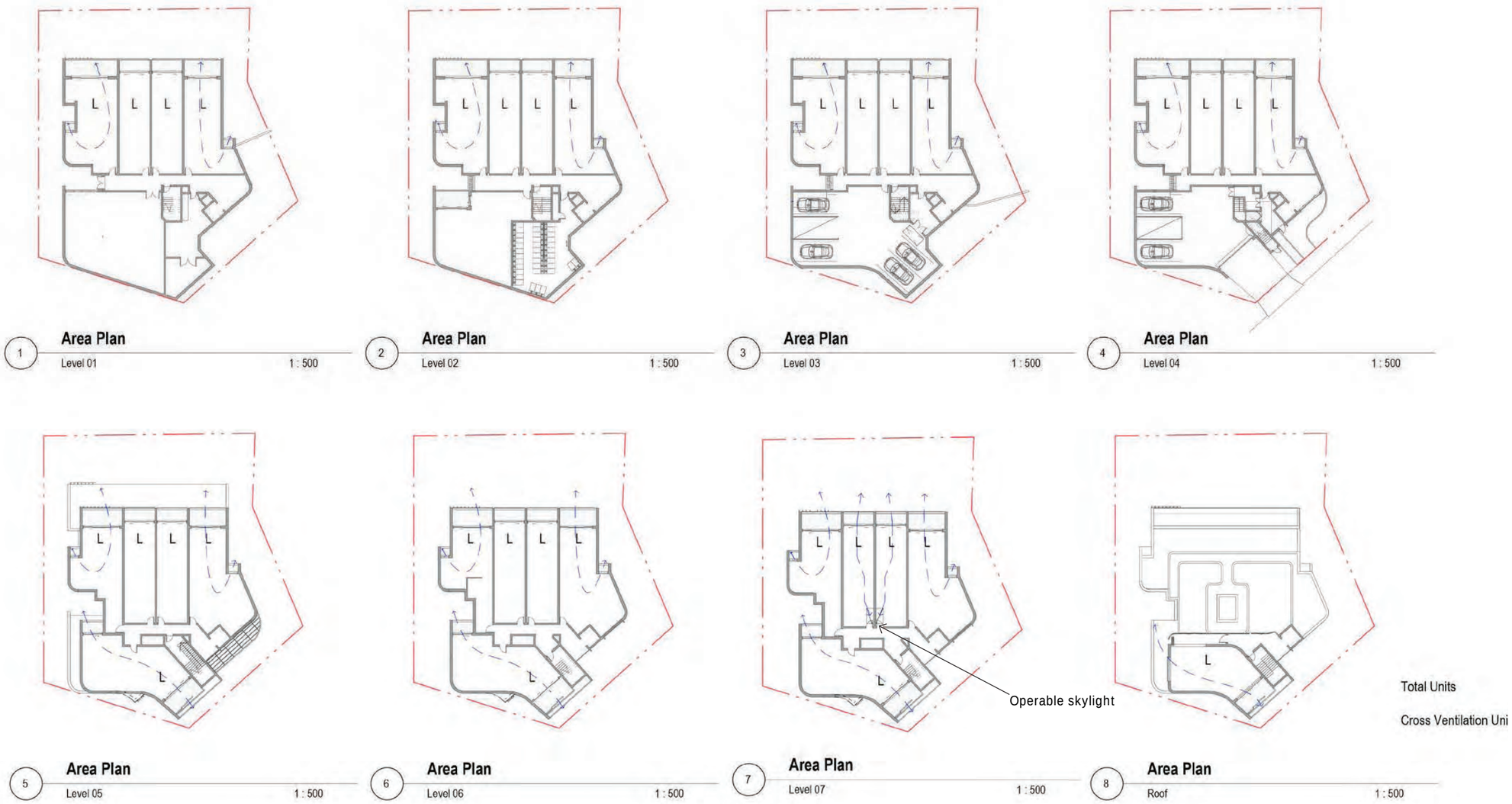


Figure 65. Cross ventilation diagrams (Antoniades Architects).

According to the information provided by Antoniades Architects, 60.6% of units (20 units of 33 units) achieve natural cross ventilation. This is in accordance with ADG guidelines.

5.9 SECTION CONCLUSION

View impacts demonstrated by the Antoniades Architects would need to be subject to further court certifiable testing to confirm the preliminary findings once access to adjoining units is granted.

View impacts are considered to be reasonable and consistent with the Desired Future Character for the area. However, whilst a change in controls for an increased development scale may introduce minor additional overshadowing impacts, the habitable rooms of the adjoining development to the south at No. 240 New South Head Road still achieves reasonable levels of solar access.

Rigorous testing and sculpting of the built form has taken place to ensure accurate mitigation and reduced levels to adjoining sites and the wider area. Other benefits include streetscape proportions, enhanced landscape character and improved aesthetic value created by re-development of the subject site.

6. URBAN DESIGN GUIDELINES

6.1 URBAN DESIGN GUIDELINES

To guide future development on the site, GMU have developed the following site specific design principles and guidelines.

MAXIMUM BUILDING HEIGHT

Building height principles:

- Achieve a building height which is harmonious to its context and the future revitalisation of the Edgecliff and Double Bay Centres.
- Achieve a built form which responds to the sloping topography.
- Achieve a scale and built form proportions to complement the streetscape profile along the northern side of New South Head Road, transitioning from the lower scale developments in Double Bay (east) to the taller forms located within the Edgecliff Town Centre area (west).
- Minimise adverse visual impacts to adjoining residential properties.
- Provide built form proportions which do not visually dominate the streetscape.

Controls:

- Provide a maximum height of 22m.
- Provide a maximum 5 storey streetwall height to New South Head Road.
- Provide floor-to-ceiling heights to meet ADG guidelines.

BUILDING SETBACKS

Setback principles are:

- Setbacks are to complement the existing and future character of the area.
- Provide adequate separation to adjoining properties to allow for good amenity outcomes and landscape opportunities.
- Provide front setbacks in response to the predominant streetscape alignments.

Controls:

- Setback distances should generally be consistent with Fig. 66
- Setback the upper levels to ensure it presents as visually recessive when viewed from adjoining properties to the north. setback for upper levels should be consistent with Fig 66.

Figure 66. Setback diagram (Antoniades Architects).

OPEN SPACE AND LANDSCAPING

Open space and landscape principles are:

- Landscape opportunities should be provided to the streetscape, side and rear boundaries to complement the landscape character of the area.
- The landscape treatment provided should complement the existing area and enhance the streetscape presentation.
- Future landscaping should contribute to the canopy coverage.
- Deep soil areas should be generous enough to allow for substantial landscape screening to side and rear boundaries, to mitigate privacy impacts and visually screen the development when viewed from adjoining properties.
- Substantial landscaping should be provided to side boundaries to improve the visual quality of the site edges.

Controls:

- Provide a minimum 6m wide deep soil area should be provided along the northern (rear) boundary to accommodate mature trees.
- Retain the large Jacaranda tree to the south eastern corner of the site as an important character element within the streetscape.
- Prioritise native species and low-water consumption plant selection.
- Provide communal open space consistent with ADG guidelines.

ACCESS

- Principles for site access should seek to:
- Consolidate the vehicular entry and minimise its visual intrusion.
- Improve the pedestrian interface where possible.
- Design pedestrian entries to complement the streetscape end minimise amenity impacts to adjoining sites.

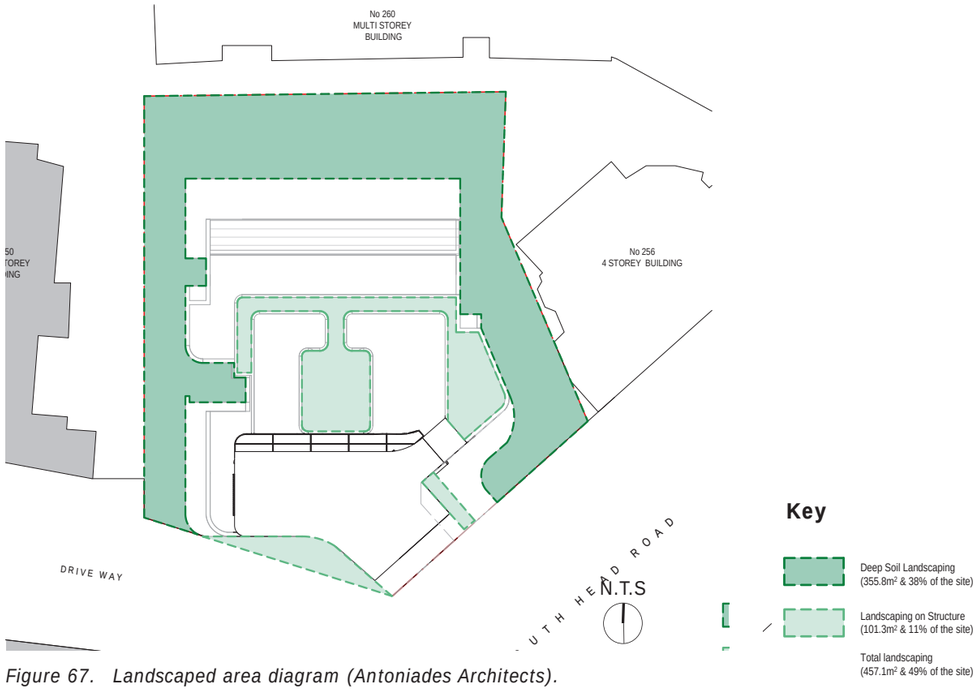


Figure 67. Landscaped area diagram (Antoniades Architects).

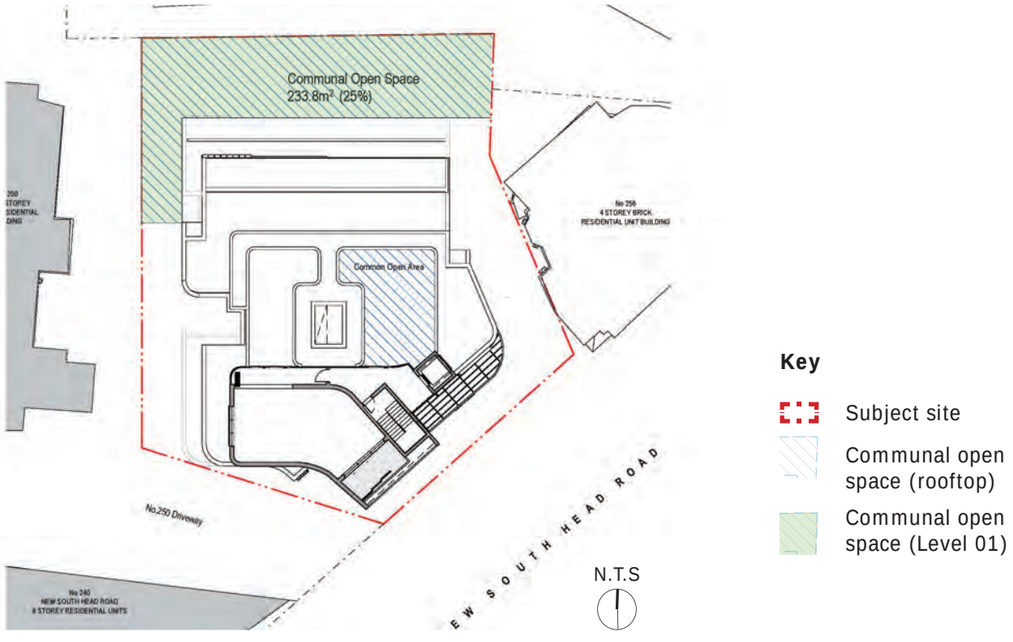


Figure 68. Indicative location of communal open space (Antoniades Architects).

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7. RECOMMENDATIONS

7.1 DEVELOPMENT OUTCOMES (INDICATIVE)

The proposed building height and FSR have the ability to facilitate a development of up to 8 storeys on the site. The indicative proposal provides 33 units with a mix of studios, one and two bedroom apartments. The proposed height and FSR also accommodate on-site facilities including bicycle spaces, storage and a gymnasium.

Indicative development outcomes are included in the Indicative Scheme (Appendix A).

7.2 PROPOSED STATUTORY CONTROLS

GMU recommend that the following statutory controls in Council's LEP be amended:

HEIGHT OF BUILDINGS

The current permissible height applicable to the site is 13.5m (WLEP). The proposal seeks to amend the maximum permissible height, by adoption of two layers of height control:

- 22m for the site generally; however incorporates
- A second height control which limits future development to a height level which will provide a compatible scale to New South Head Road. Please refer the Planning Proposal Report by GSA Planning (March 2020).

FLOOR SPACE RATIO

The current maximum Floor Space Ratio (FSR) applicable to the site is 1.3:1. The proposal seeks to amend the control to allow for the following:

- 2.6:1 across the site.

To ensure the capacity for future development to provide a compatible scale to New South Head Road, we propose a second layer of height control is adopted for the site. Please refer to Planning Proposal Report prepared by GSA Planning (March 2020).

7.3 CONCLUSIONS

GMU and the project team have undertaken extensive analysis of the site and the area. We have adopted an evidence-based approach to our investigations of the likely future development of the Edgecliff and Double Bay localities, to understand the role of the site relative to both local centres.

GMU consider it appropriate to provide a development scale on the site which responds to recent approvals nearby and thereby improve the streetscape profile by transtioning to the taller developments close to the Edgecliff centre and Edgecliff train station. The proposed height responds to the sloping site, delivering a sympathetic response to the natural terrain and the unique landforms characterising the area. The proposal aims to maximise the site's potential and regenerate the existing facility whilst achieving a good contextual fit.

Future redevelopment of the site will complement local strategic aims, delivering diverse housing close to the public transport hub and unique life style amenity.

GMU
Urban Design & Architecture

Project 252 New South Head Road, Double Bay
Job number 19121
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8. APPENDIX A - INDICATIVE SCHEME